MAP REFERENCES:

1. "SURVEY OF NO. 446 SARATOGA ROAD", PREPARED BY FREDERICK J. METZGER NYS LLS NO. 49617, DATED NOVEMBER 14, 1996.

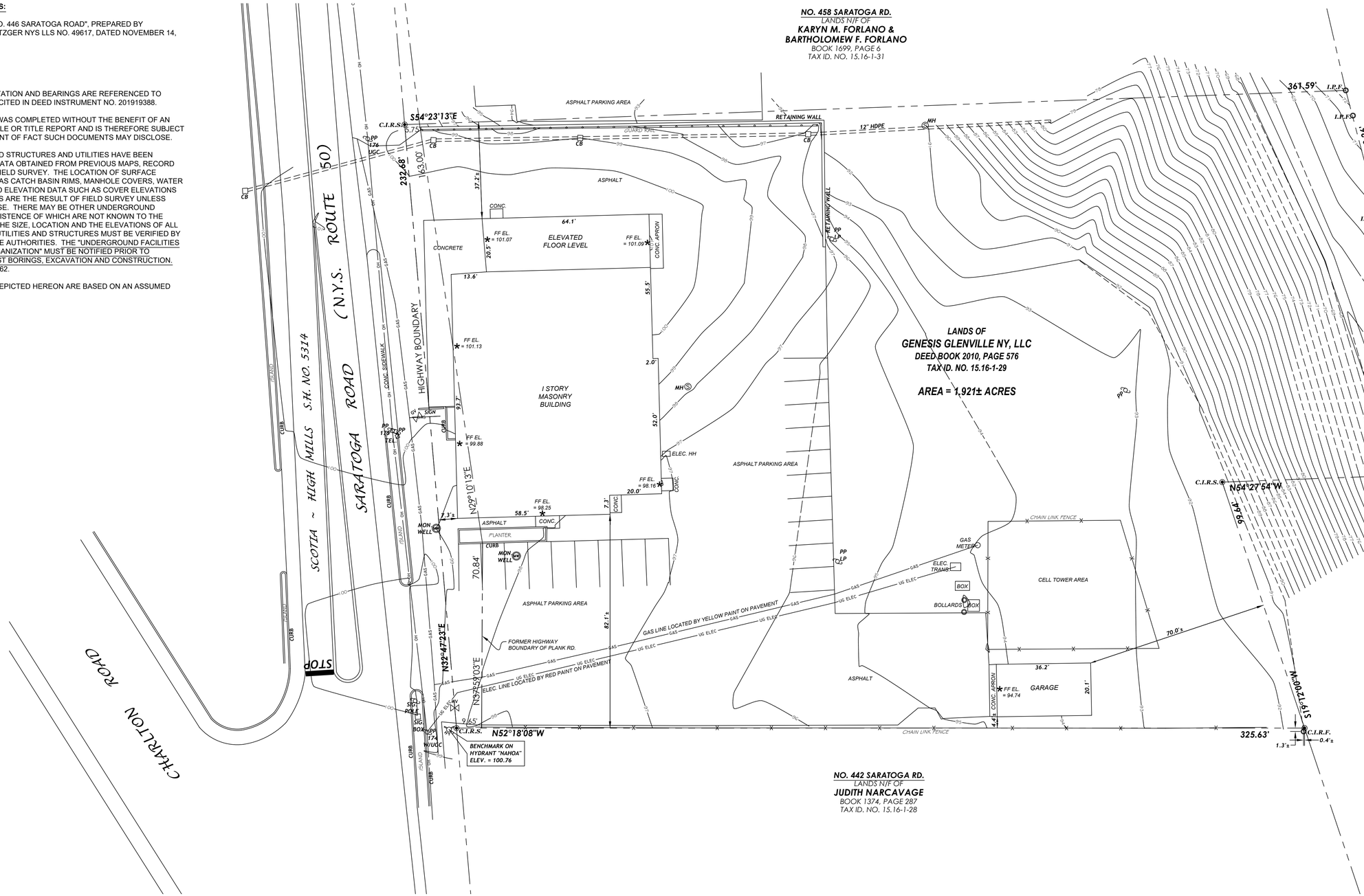
MAP NOTES:

1. NORTH ORIENTATION AND BEARINGS ARE REFERENCED TO INFORMATION RECITED IN DEED INSTRUMENT NO. 201919388.

2. THIS SURVEY WAS COMPLETED WITHOUT THE BENEFIT OF AN ABSTRACT OF TITLE OR TITLE REPORT AND IS THEREFORE SUBJECT TO ANY STATEMENT OF FACT SUCH DOCUMENTS MAY DISCLOSE.

3. UNDERGROUND STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM DATA OBTAINED FROM PREVIOUS MAPS, RECORD DRAWINGS AND FIELD SURVEY. THE LOCATION OF SURFACE FEATURES SUCH AS CATCH BASIN RIMS, MANHOLE COVERS, WATER VALVES, ETC. AND ELEVATION DATA SUCH AS COVER ELEVATIONS AND PIPE INVERTS ARE THE RESULT OF FIELD SURVEY UNLESS NOTED OTHERWISE. THERE MAY BE OTHER UNDERGROUND UTILITIES, THE EXISTENCE OF WHICH ARE NOT KNOWN TO THE UNDERSIGNED. THE SIZE, LOCATION AND THE ELEVATIONS OF ALL UNDERGROUND UTILITIES AND STRUCTURES MUST BE VERIFIED BY THE APPROPRIATE AUTHORITIES. THE "UNDERGROUND FACILITIES PROTECTIVE ORGANIZATION" MUST BE NOTIFIED PRIOR TO CONDUCTING TEST BORINGS, EXCAVATION AND CONSTRUCTION. CALL 1-800-962-7962.

4. ELEVATIONS DEPICTED HEREON ARE BASED ON AN ASSUMED DATUM.



LEGEND

(DC)	DEED CALL
W/W	WITH WIRE
C.I.R.S	CAPPED IRON ROD SET
I.P.F.	IRON PIPE FOUND
C.I.R.F.	CAPPED IRON ROD FOUND
I.R.F.	IRON ROD FOUND
116.00-2-39	TAX MAP SECTION-BLOCK-LOT
B.769 P.986	DEED BOOK & PAGE
N/F	NOW OR FORMERLY
(176.5)	DEED INFORMATION
OH	OVERHEAD UTILITY LINES
	FENCE

GRAPHIC SCALE

20 0 10 20 40 80

(IN FEET)
1 inch = 20 ft.

SHEET 1 OF 1

TOPOGRAPHIC SURVEY OF LANDS OF GENESIS GLENVILLE NY, LLC

DEED BOOK 2010, PAGE 576 ~ TAX ID. NO. 15.16-1-29
NO. 446 SARATOGA ROAD
MUNICIPALITY OF GLENVILLE, SCHENECTADY COUNTY, NEW YORK

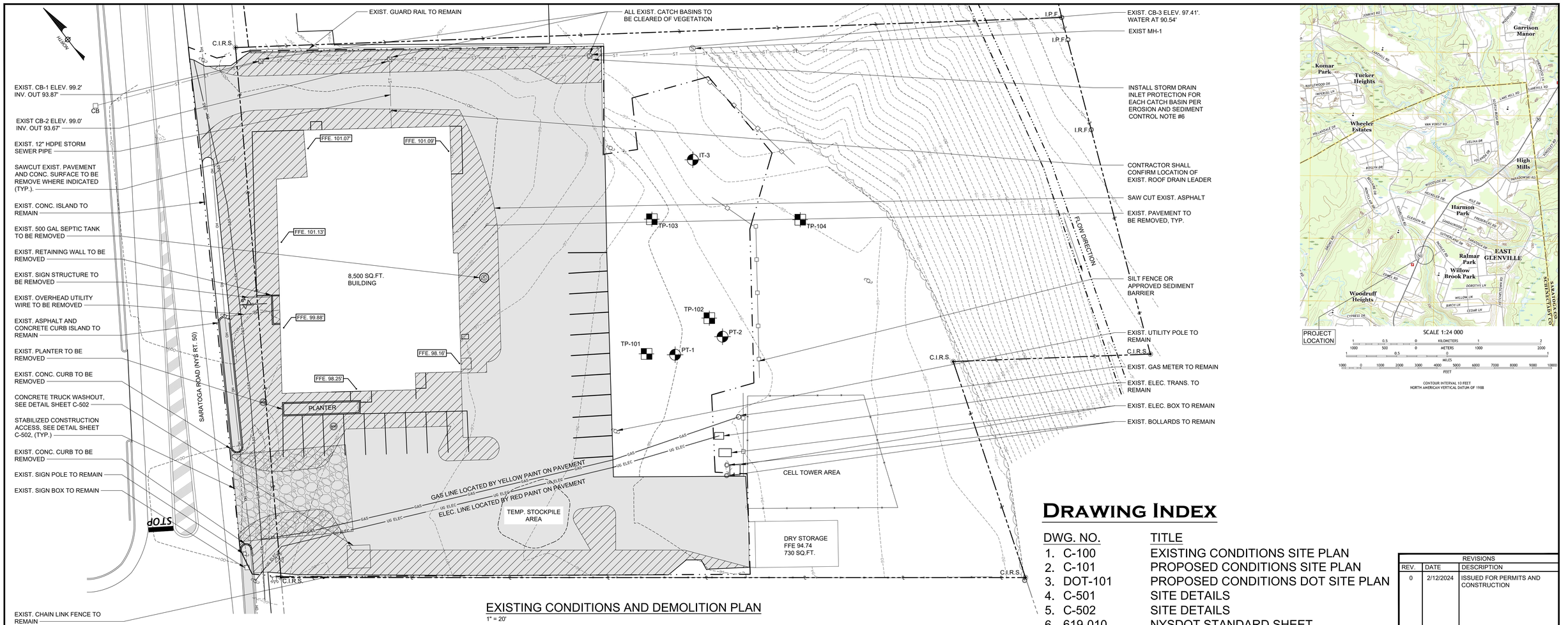
DATE	DESCRIPTION OF WORK	DWG.	CHECK	INITIALS

TOWN OF: GLENVILLE
COUNTY OF: SCHENECTADY, N.Y.
SCALE: 1" = 20'
DATE: JULY 6, 2022
PROJ. NO.: 22027
DWG. NO.: 22-043
DRAWN BY: J.R.

"UNAUTHORIZED ALTERATION OR ADDITION TO A SURVEY MAP BEARING A LICENSED LAND SURVEYOR'S SEAL IS A VIOLATION OF SECTION 7209, SUBDIVISION 2, OF THE NEW YORK STATE EDUCATION LAW."

"ORIGINAL COPY OF THIS SURVEY MAP MARKED WITH AN IMPRESSED SEAL OF THE LAND SURVEYOR'S EMBOSSED SEAL SHALL BE CONSIDERED TO BE VALID TRUE COPIES."

NORTHEAST LAND SURVEY & LAND DEVELOPMENT CONSULTANTS, P.C.
P.O. BOX 2140, MALTA, N.Y. 12020, PARADE GROUND VILLAGE
PH: (518) 899-7339 FAX: (518) 899-7340
EMAIL: NORTHEAST@NELANDSURVEY.COM
NORTHEAST LAND SURVEY & LAND DEVELOPMENT CONSULTANTS, P.C.



EXISTING CONDITIONS AND DEMOLITION PLAN

1" = 20'

0 10 20 40 60

UNITS: FEET

CONTOUR INTERVAL 10 FEET
NORTH AMERICAN VERTICAL DATUM OF 1988

LEGEND

- EXIST. MINOR CONT.
- EXIST. MAJOR CONT.
- PROP. CONTOUR
- PROPERTY LINE
- SILT FENCE
- UNDERGROUND GAS LINE
- UNDERGROUND ELECTRIC LINE
- UNDERGROUND SEWER LINE
- UNDERGROUND STORM WATER LINE
- OVERHEAD UTILITY WIRE
- GUARD RAIL
- SETBACK
- CHAIN LINK FENCE
- TEST PIT LOCATION
- PERC. TEST LOCATION
- PROPOSED SPOT ELEVATION
- UTILITY POLE
- GAS VALVE
- MONITOR WELL MANHOLE
- WATER VALVE
- WATER HYDRANT
- SEWER MANHOLE
- CATCH BASIN
- E.V. CHARGE STATION

TEST PIT AND INFILTRATION TEST LOG

TEST PIT TP-101	0"-10"	TOPSOIL - DRY, BROWN, LOAMY FINE SAND, FRIABLE
	10"-26"	DRY, LIGHT BROWN/TAN, FINE SAND, FRIABLE TO FIRM, ORANGE/BROWN FINE SAND
	26"-72"	DRY, BROWN/GRAY, MEDIUM SAND, FRIABLE
TEST PIT TP-102	0"-6"	TOPSOIL - DRY, BROWN/DARK BROWN, FRIABLE, LOAMY SAND
	6"-23"	DRY, LIGHT BROWN, FRIABLE TO FIRM, FINE SAND, PIECES OF ASPHALT PRESENT AT 8" AND 12"
	23"-43"	DRY, TAN, FRIABLE FINE SAND
	43"-71"	DRY, LIGHT BROWN/TAN, FINE-MEDIUM SAND
TEST PIT TP-103	0"-11"	TOPSOIL - DRY, BROWN, FRIABLE, LOAMY SAND
	11"-27"	DRY, LIGHT ORANGE/BROWN, FINE SAND, FRIABLE, PIECES OF ASPHALT WITHIN 12"
	27"-36"	DRY, LIGHT GRAY, FINE SAND, FRIABLE TO FIRM
	36"-76"	DRY, FRIABLE MEDIUM SAND W/ VARIABLE COLORS (REDDISH TAN/LIGHT BROWN AND LIGHT BROWN/ORANGE/BLACK AND WHITE)
TEST PIT TP-104	0"-4"	DRY, BROWN LOAMY SAND, FRIABLE
	4"-50"	DRY, LIGHT BROWN, FRIABLE, FINE-MEDIUM SAND, PIECES OF ASPHALT WITHIN 24"

NO MOTTLES OR EVIDENCE OF GROUNDWATER OR BEDROCK ENCOUNTERED IN ANY TEST PIT.
TEST PITS WERE OBSERVED BY ERIK SANDBLOM, PE OF SRA ENGINEERS ON 8/24/2022

PERCOLATION TEST	PT-1	STABILIZED PERCOLATION RATE = 1.5min./inch TEST AT ±36" BELOW EXISTING GRADE
PERCOLATION TEST	PT-2	STABILIZED PERCOLATION RATE = 7.0min./inch TEST AT ±32" BELOW EXISTING GRADE
INFILTRATION TEST	IT-3	STABILIZED INFILTRATION RATE = 24inch/hour TEST AT ±32" BELOW EXISTING GRADE

PERCOLATION TEST WAS PERFORMED BY SRA ENGINEERS ON 8/24/2022

GENERAL NOTES

1. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL STAKE OUT ALL IMPROVEMENTS AND VERIFY GRADES AND ELEVATIONS. AND DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER.
2. CONTRACTOR SHALL STRIP ALL TOPSOIL IN AREAS TO BE RE-GRADED AND STOCKPILED FOR LATER USE.
3. THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE DETERMINED BY THE CONTRACTOR IN THE FIELD. THE CONTRACTOR SHALL CONDUCT OPERATIONS AND TAKE THE NECESSARY PRECAUTIONS SUCH THAT INTERFERENCE WITH OR DAMAGE TO EXISTING UTILITIES IS PREVENTED. THE CONTRACTOR SHALL COORDINATE WITH "DIG-SAFE" TO HAVE ALL UNDERGROUND UTILITIES LOCATED PRIOR TO COMMENCING EXCAVATION WORK. IF THE CONTRACTOR DAMAGES AN EXISTING UTILITY, HE SHALL COMMENCE WORK TO REPAIR THAT SERVICE IMMEDIATELY AND ALL COSTS ASSOCIATED WITH SUCH REPAIR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
4. THE CONTRACTOR SHALL COMPLY WITH ALL REQUIRED PERMITS AND ASSOCIATED CONDITIONS.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR DEWATERING AND MAINTENANCE OF SURFACE DRAINAGE DURING THE DURATION OF THE WORK.
6. CONTRACTOR IS RESPONSIBLE FOR EMPLOYING AND MAINTAINING ALL TRAFFIC CONTROL AND SAFETY MEASURES DURING CONSTRUCTION.

SURVEY NOTES

1. BASE MAPPING DEVELOPED FROM SURVEY DATA FROM "TOPOGRAPHIC SURVEY OF LANDS OF GENESIS GLENVILLE, NY, LLC. DEED BOOK 2010, PAGE 576 - TAX I.D. NO. 15.16-1-29. NO. 446 SARATOGA ROAD MUNICIPALITY OF GLENVILLE, SCHENECTADY COUNTY, NEW YORK" BY NORTHEAST LAND SURVEY & LAND DEVELOPMENT CONSULTANTS, P.C., DATED JULY 6, 2022 (PROJECT NUMBER 22/027).
2. REFER TO ORIGINAL SURVEY FOR ADDITIONAL NOTES.
3. THIS SHALL NOT BE USED AS A BOUNDARY OR CERTIFIED SURVEY OF THE PROPERTY.
4. TOPOGRAPHIC CONTOURS OUTSIDE OF THE PROPERTY BOUNDARIES WERE ESTIMATED BASED UPON REVIEW OF 20-FOOT CONTOURS FROM TOPOGRAPHIC BASE MAPPING PROVIDED BY WARREN COUNTY GIS PROGRAM.

EROSION & SEDIMENT CONTROL NOTES

1. ALL SILT FENCES SHALL BE IN PLACE AT LOCATIONS INDICATED IN ACCORDANCE WITH THE DETAILS BEFORE COMMENCEMENT OF ANY DISTURBANCE OF EXISTING GROUND SURFACE.
2. ALL FINAL EXPOSED CUT AND FILL EARTHWORK SURFACES SHALL BE COVERED WITH MIN. 4" OF TOPSOIL, HYDROSEED PER SPECIFICATION, UNLESS NOTED TO BE COVERED BY PAVEMENT, OR STRUCTURES.
3. ALL AREAS UNWORKED FOR MORE THAN 14 DAYS MUST BE SEEDED WITH ANNUAL RYE GRASS AND PROTECTED WITH STRAW MULCH.
4. AFTER EVERY STORM EVENT IN EXCESS OF 1/2" RAINFALL, INSPECT ALL SILT FENCES. REMOVE ACCUMULATED MATERIAL, FILL ERODED AREAS AND RESET SILT FENCES.
5. ENCLOSE ALL STOCKPILES WITH SILT FENCE.
6. STORM DRAIN INLET PROTECTION SHALL BE INSTALLED ON ALL EXISTING CATCH BASINS ON SITE. INLET PROTECTION SHALL BE INSTALLED IN ACCORDANCE WITH THE "NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL" SECTION 5.57 "STANDARDS AND SPECIFICATIONS FOR STORM DRAIN INLET PROTECTION"
7. EROSION CONTROL MEASURES SHALL CONFORM WITH THE REQUIREMENTS OF NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL AND THE SITE SPECIFIC STORMWATER POLLUTION PREVENTION PLAN (SWPPP) FOR THIS PROJECT.
8. THE CONTRACTOR SHALL PROVIDE A DUMPSTER ON-SITE FOR LEGAL DISPOSAL OF DEBRIS, GARBAGE AND LITTER.
9. SILT FENCES AND/OR SILT SOCKS SHALL REMAIN IN PLACE UNTIL A STABLE GROWTH OF TURF IS PRESENT AT ALL DISTURBED AREAS.
10. THE CONTRACTOR SHALL PROVIDE AN APPROVED SECONDARY CONTAINMENT SYSTEM FOR ALL FUEL AND PETROLEUM PRODUCTS TEMPORARILY STORED ON THE SITE.
11. ANCHORED STABILIZED CONTROL MATTING SHALL BE INSTALLED IN ACCORDANCE WITH "NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL" SECTION 4.5 "STANDARDS AND SPECIFICATIONS FOR ANCHORED STABILIZATION MATTING"
12. ALL AREAS OF EXIST. IMPERVIOUS SURFACE THAT WILL BE REVERTED TO PERVIOUS SHALL IMPLEMENT SOIL RESTORATION IN ACCORDANCE WITH THE FOLLOWING PROCEDURE:
    - APPLY 3 INCHES OF COMPOST OVER SUBSOIL
    - TILL COMPOST INTO SUBSOIL TO A DEPTH OF AT LEAST 12 INCHES USING A CAT-MOUNTED RIPPER, TRACTOR-MOUNTED DISC, OR TILLER, MIXING, AND CIRCULATING AIR AND COMPOST INTO SUBSOIL.
    - ROCK-PICK UNTIL UPLIFTED STONE/ROCK MATERIALS OF FOUR INCHES AND LARGER SIZE ARE CLEANED OFF THE SITE
    - APPLY TOPSOIL TO A DEPTH OF 6 INCHES
    - VEGETATE AS REQUIRED BY APPROVED PLAN

    TILLING (STEP 2 ABOVE) SHOULD NOT BE PERFORMED WITHIN THE DRIP LINE OF ANY EXISTING TREES OR OVER UTILITY INSTALLATIONS THAT ARE WITHIN 24 INCHES OF THE SURFACE.

DRAWING INDEX

REVISIONS		
REV.	DATE	DESCRIPTION
0	2/12/2024	ISSUED FOR PERMITS AND CONSTRUCTION

STATE OF NEW YORK
ERIK C.F. SANDBLOM
NO. 076451
LICENSED PROFESSIONAL ENGINEER

SRA ENGINEERS
Evergreen Professional Park
453 Dixon Road, Suite 7, Bldg. 3
Queensbury, New York 12804
(518) 761-0417, FAX: (518) 761-0513

SCALE:	AS NOTED	DRAWN BY:	AMD / RAS
DATE:	2/7/2024	ENG. BY:	AMD / RAS
PROJ. NO:	22-960	CHK'D BY:	ES

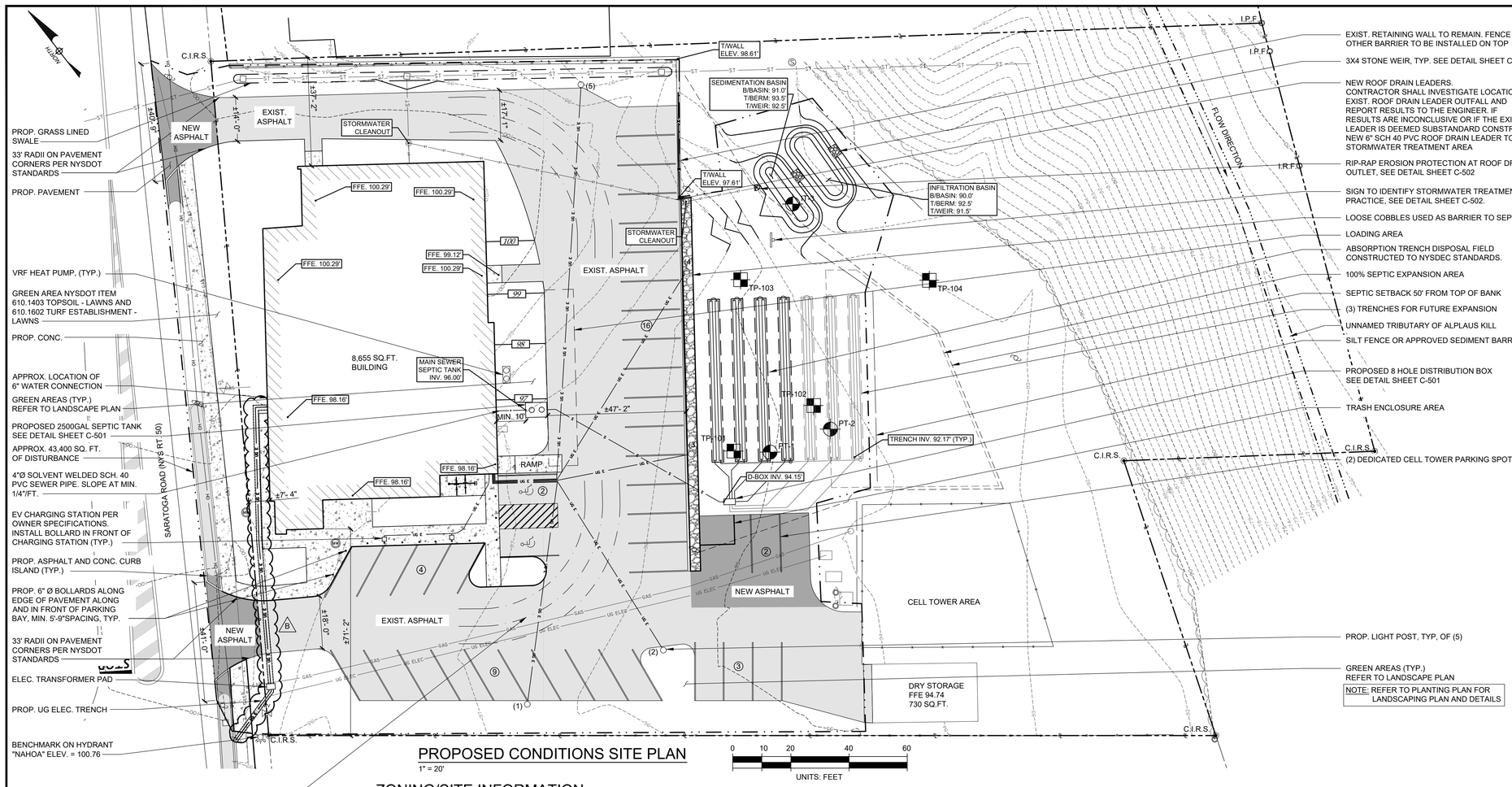
CLIENT NAME
446 SARATOGA ROAD LLC
446 SARATOGA RD
SCHENECTADY, NY, 12302

DRAWING TITLE
EXISTING CONDITIONS SITE PLAN
446 SARATOGA ROAD
GLENVILLE, NY, 12302

DRAWING NO. C-100

SHT. 1 OF 8

REV. 0



PROPOSED CONDITIONS SITE PLAN

1" = 20'

0 10 20 40 60

UNITS: FEET

LEGEND

- EXIST. MINOR CONT.
- EXIST. MAJOR CONT.
- PROP. CONTOUR
- PROPERTY LINE
- SILT FENCE
- UNDERGROUND GAS LINE
- UNDERGROUND ELECTRIC LINE
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- MONITOR WELL MANHOLE
- WATER VALVE
- WATER HYDRANT
- SEWER MANHOLE
- CATCH BASIN
- E.V. CHARGE STATION

ZONING/SITE INFORMATION

ZONING CLASSIFICATION: GENERAL BUSINESS
PROPOSED USE: COMMERCIAL: GENERAL OFFICE, VOCATIONAL
MINIMUM REQUIRED AREA: 40,000 SQ. FT.
TAX MAP NO. 15.16-1-29
LOT SIZE: 1.92 ACRES (83,635 SQ. FT.)
TOTAL PROPOSED DISTURBED AREA: +/- 43,400 SQ. FT.
PROPOSED NEW BUILDING AREA: 155 SQ. FT., ±2% OF EXIST. STRUCTURE
TOTAL ADDITIONAL IMPERVIOUS AREA: (-7,732) SQ. FT.
TOTAL RESULTING IMPERVIOUS AREA OVER ENTIRE SITE: 30,965 SQ FT. (37.0%)
PARKING REQUIREMENTS:
PARKING SPACES REQUIRED: 24 (SEE SHEET PB-100 FOR MORE DETAILS)
PARKING SPACES PROVIDED: 34 PLUS TWO (2) DEDICATED CELL TOWER SPACES

	SETBACK REQUIREMENTS		
DIMENSION	REQUIRED	EXISTING	PROPOSED
FRONT	35 FT.	7.3 FT.	7.3 FT.
SIDE YARD	30 FT.	37 FT.	37 FT.
REAR YARD	40 FT.	215 FT.	215 FT.
HEIGHT (MAX)	35 FT.	25 FT.	25 FT.
WATERCOURSE BUFFER (TOP OF BANK)	50FT.	>50 FT.	>50 FT.
COVERAGE (MAX)	30%	11.0%	11.3%

STORMWATER SYSTEM NOTES

1. MAXIMUM DISTURBED SITE AREA FOR THIS PROJECT EXCEEDS ONE ACRE. THEREFORE THE PROJECT SHALL SEEK COVERAGE UNDER THE NYSDEC SPDES GENERAL PERMIT FOR STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITY AND REQUIRES A STORMWATER PERMIT FROM THE TOWN OF GLENVILLE, NEW YORK.
2. THE BASIS OF DESIGN FOR THE STORMWATER TREATMENT SYSTEM AND CALCULATIONS ARE PROVIDED IN THE SITE SPECIFIC STORMWATER POLLUTION PLAN (SWPPP) PREPARED FOR THE PROJECT. REFER TO THE SWPPP FOR ADDITIONAL REQUIREMENTS.
3. INFILTRATION BASIN SEED MIX: VERMONT WET MEADOW AND DETENTION BASIN MIX OR EQUAL.

   SPECIES COMPOSITION: SWITCHGRASS (PANICUM VIRGATUM), VIRGINIA WILD RYE (ELYMUS VIRGINICUS), RED FESCUE (FESTUCA RUBRA), FOX SEDGE (CAREX VULPINOIDEA), WOOLGRASS (SCIRPUS CYPERINUS), GREEN BULRUSH (SCIRPUS ATROVIRENS), NODDING BUR-MARIGOLD (BIDENS CERNUA), BONESET (EUPATORIUM PERFOLIATUM), JOE-PYE WEED (EUPATORIADELPHUS MACULATUS), SOFT RUSH (JUNCUS EFFUSUS), SENSITIVE FERN (ONOCLEA SENSIBILIS), BLUE VERVAIN (VERBENA HASTATA), NEW ENGLAND ASTER (SYMPHYOTRICHUM NOVA-ANGLIAEA).

   APPLICATION RATE: 35 LBS/ACRE
4. INFILTRATION BASIN AND SWALE MAINTENANCE: PERIODICALLY MAINTAIN THE INFILTRATION BASIN AND SWALES IN ACCORDANCE WITH THE FOLLOWING:
   - REMOVE ANY ACCUMULATED SEDIMENT, TRASH, AND DEBRIS
   - REMOVE DEAD VEGETATION, TRIM LIVE VEGETATION TO A MAXIMUM HEIGHT OF 12" AND REMOVE WEEDS
   - REPAIR INLET EROSION/DAMAGE
   - LOOSEN, AERATE, OR REPLACE SOILS IN THE INFILTRATION BASIN IF IT DOES NOT INFILTRATE WITHIN 48 HOURS
5. AN ADDITIONAL TEST PIT SHALL BE DUG BY THE CONTRACTOR AND ASSESSED BY SRA ENGINEERS PRIOR TO CONSTRUCTION OF THE SEDIMENTATION AND INFILTRATION BASINS. THE PURPOSE OF THIS TEST PIT IS TO IDENTIFY DEPTH OF GROUNDWATER AND CONFIRM THE MINIMUM 4' SEPARATION TO GROUNDWATER WILL BE MET.

WASTEWATER TREATMENT SYSTEM NOTES

1. THE ON-SITE SUBSURFACE WASTEWATER TREATMENT SYSTEM SHALL BE A GRAVITY FED GRAVELESS GEOTEXTILE SAND FILTERS IN A TRENCH CONFIGURATION UTILIZING ADVANCED ENVIRO-SEPTIC (AES) PIPE AS MANUFACTURED BY PRESBY ENVIRONMENTAL, INC., WHITEFIELD, NH. THE BASIS OF DESIGN FOR THE SYSTEM IS AS FOLLOWS:

DESIGN WASTEWATER FLOW RATE	720 GPD (48 x 15) (1,500 gpd MAX FOR FUTURE EXPANSION)
NATIVE PERCOLATION RATE	6-7 min/in
DESIGN PERCOLATION RATE	6-7 min/in
SEPTIC TANK MIN. CAPACITY	2,500 gal
SEPTIC TANK CAPACITY PROVIDED	2,500 gal (NO SPAS OR GARBAGE GRINDERS)
NATIVE APPLICATION RATE	1.00 gal/day/sf
PRESBY MANUAL MIN TRENCH LENGTH	250 ft
NUMBER OF TRENCHES @ 61' (250 ft / 61' = 4.09)	5 TRENCHES @ 61'
TOTAL AES PIPE (61 ft - 1 ft) x 5 x 2	600 ft
MIN LENGTH AES PIPE PER NYSDEC MANUAL (720 gpd / 1.0 sf/gpd / 2)	360 ft
AES PIPE LENGTH PROVIDED (55 ft - 1 ft) x 4 x 2	432 ft
PLUMBING FIXTURES	MAX. 1.6 Gal./Flush WATER CLOSET

2. THE AES PIPE WASTEWATER TREATMENT SYSTEM SHALL BE INSTALLED BY A CONTRACTOR CERTIFIED BY PRESBY ENVIRONMENTAL, INC. TO PERFORM RESIDENTIAL SYSTEM INSTALLATIONS IN NEW YORK STATE.
3. ALL WASTEWATER TREATMENT SYSTEM COMPONENTS SHALL BE IN STRICT ACCORDANCE WITH THE SYSTEM DESIGN INFORMATION INDICATED ON THESE DRAWINGS, ADVANCED ENVIRO-SEPTIC (AES) INSTALLATION GUIDELINES AND WITH ALL N.Y.S. DEPT. OF HEALTH AND TOWN OF GLENVILLE REQUIREMENTS.
4. THE DISCHARGE OF WASTE FROM GARBAGE GRINDERS, SPA TUBS, SUMP PUMPS, ROOF DRAINS, WATER TREATMENT SYSTEM BACKWASH AND NON-CONSERVING WATER FIXTURES IS PROHIBITED.
5. FILL OVER THE ABSORPTION TRENCHES SHALL BE CLEAN SANDY LOAM, FREE OF ORGANIC MATTER AND STONES LARGER THAN 2 IN. IN ANY DIMENSION.
6. NO HEAVY EQUIPMENT OR VEHICULAR TRAFFIC SHALL BE ALLOWED WITHIN THE LIMITS OF THE ABSORPTION FIELD AFTER FILL MATERIAL HAS BEEN PLACED BOTH DURING OR AFTER CONSTRUCTION.
7. PLACEMENT OF BACKFILL, AES PIPE AND FILTER FABRIC SHALL BE AS SHOWN.
8. TOPSOIL AND SEED THE ABSORPTION FIELD AREA AND ALL OTHER DISTURBED AREAS IMMEDIATELY AFTER CONSTRUCTION.
9. ALL PLUMBING WORK SHALL BE COMPLETED IN STRICT ACCORDANCE WITH ALL REQUIREMENTS OF THE BUILDING CODE OF NEW YORK STATE.
10. UPON COMPLETION OF CONSTRUCTION THE CONTRACTOR SHALL PREPARE A MAP OF AS-BUILT CONDITIONS INDICATING A MINIMUM OF TWO TIE DIMENSIONS FROM ADJACENT CORNERS OF THE BUILDING TO THE SEPTIC TANK INLET AND OUTLET COVERS, THE DISTRIBUTION BOX AND EACH CORNER OF THE LEACH FIELD.

WASTEWATER TREATMENT SYSTEM NOTES (CONT.)

11. THE SEPTIC TANK SHALL BE A PRECAST CONCRETE NON-TRAFFIC TYPE UNIT AS MANUFACTURED BY THE FORT MILLER CO., INC., WITH SANITARY TEES OR BAFFLES AT INLETS AND OUTLETS. AN EFFLUENT FILTER SHALL BE PROVIDED AT THE OUTLET. MINIMUM TANK CAPACITY SHALL BE AS SHOWN. FOR TWO-PIECE TANKS, PROVIDE SEALANT PER MANUFACTURER'S INSTRUCTIONS BETWEEN THE TANK SECTIONS. THOROUGHLY CLEAN JOINT IMMEDIATELY PRIOR TO INSTALLING SEALANT. IF AIR TEMPERATURE IS BELOW 65°F., WARM SEALANT TO A PLIABLE CONDITION. SET TANKS LEVEL ON NO. 2 CRUSHED STONE AS INDICATED.
12. FILL OVER AND BETWEEN THE AES TRENCHES SHALL BE CLEAN SANDY LOAM, FREE OF ORGANIC MATTER AND STONES LARGER THAN 2 IN. IN ANY DIMENSION.
13. EXISTING SOIL SURFACE WITHIN THE TREATMENT SYSTEM AREA SHALL BE REMOVED OF THE ORGANIC LAYER (GRASS, LEAVES, ETC.) AND BE SCARIFIED PRIOR TO PLACING SANDY LOAM FILL AND SYSTEM SAND TO THE LINES AND GRADES INDICATED.
14. SAND FILL SHALL BE CLEAN GRANULAR MATERIAL AND SHALL BE FREE OF ORGANIC MATTER AND STONES LARGER THAN 2 IN. IN ANY DIMENSION. THE IN-PLACE PERCOLATION RATE OF THE SANDY LOAM FILL SHALL BE IN THE RANGE OF 5-10 MIN/IN. AND NOT MORE THAN 3% PASSING THE #200 SIEVE AS VERIFIED BY THE ENGINEER PRIOR TO PLACEMENT OF THE SYSTEM SAND AND CONSTRUCTION OF THE REMAINDER OF THE TREATMENT SYSTEM. SYSTEM SAND MAY BE USED IN PLACE OF SAND FILL.
15. PLACEMENT OF SANDY LOAM FILL, SYSTEM SAND AND AES PIPE SHALL BE AS SHOWN AND CARE SHALL BE TAKEN TO AVOID INCLUSION OF FINE GRAINED SOIL AND WASTE MATERIAL IN THE SAND AND PIPING.
16. SYSTEM SAND SHALL CONFORM TO THE FOLLOWING PERCENTAGE AND QUALITY RESTRICTIONS:
   - NO STONES OVER 3/4" IN DIAMETER.
   - 35% MAX. RETAINED BY A #10 SIEVE
   - 40-90% RETAINED BY A #35 SIEVE
   - MAXIMUM 2% OF TOTAL SAND MAY PASS THROUGH #200 SIEVE
   - ASTM C-33 SAND MAY BE ACCEPTABLE PROVIDING NO MORE THAN 2% PASSES A #200 SIEVE.
17. TOPSOIL AND SEED THE DISPOSAL FIELD AREA AND ALL OTHER DISTURBED AREAS IMMEDIATELY AFTER CONSTRUCTION.
18. NOTIFY THE ENGINEER FOR INSPECTION AT THE FOLLOWING PHASES OF CONSTRUCTION:
   - COMPLETION OF AES PIPE INSTALLATION, PRIOR TO BACKFILLING WITH THE LATERALS EXPOSED.
19. SUBMITTALS: THE CONTRACTOR SHALL SUBMIT MIN. (3) COPIES OF SHOP DRAWINGS AND SUBMITTALS FOR THE FOLLOWING ITEMS FOR REVIEW BY THE ENGINEER. IN LIEU OF (3) HARD COPIES, (1) COPY OF SHOP DRAWINGS AND SUBMITTALS MAY BE TRANSMITTED ELECTRONICALLY. NO FABRICATION OF THESE ITEMS SHALL BE PERMITTED UNTIL THE SUBMITTALS HAVE BEEN REVIEWED AND ACCEPTED.

   A. PRECAST CONCRETE SEPTIC TANK SHOP DRAWINGS
   B. ADVANCED ENVIRO-SEPTIC (AES) PIPE
   C. SYSTEM SAND SIEVE ANALYSIS
   D. SAND FILL SIEVE ANALYSIS AND BORROW PIT PERCOLATION TEST RESULTS

EARTHWORK NOTES

1. EXCAVATION SHALL BE TO ELEVATIONS INDICATED WITH A TOLERANCE OF PLUS/MINUS 1".
2. SUBBASE COURSE MATERIAL SHALL CONFORM TO NYS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS SECTION 304, TYPE 2 AND SHALL CONFORM TO MIN 95% MODIFIED PROCTOR DENSITY. BACKFILL MATERIAL SHALL BE COMPACTED ON SITE SOIL MATERIAL.
3. EMBANKMENT FILL SHALL CONFORM WITH ALL REQUIREMENTS OF NYSDOT SPEC. SECTION 203. MAXIMUM ALLOWABLE LOOSE LIFTS FOR EMBANKMENTS SHALL BE 12 INCH AND SHALL BE COMPACTED TO NOT LESS THAN 95% OF THE MAXIMUM DENSITY AS ESTABLISHED THROUGH THE STANDARD PROCTOR COMPACTION TEST PER ASTM D698.
4. MATERIAL STOCKPILES, IF REQUIRED, SHALL BE LOCATED WHOLLY WITHIN THE WORK AREA. ADDITIONAL SILT FENCES SHALL BE PROVIDED AT THE BASE OF ALL STOCKPILES AS DIRECTED IN THE FIELD BY THE ENGINEER.
5. ALL TRENCHES AND OTHER EXCAVATION SIDE SLOPES INDICATED ON THE DRAWINGS ARE DIAGRAMMATIC ONLY AND ARE NOT INTENDED TO INDICATE A STABLE EXCAVATION SLOPE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ADEQUACY AND STABILITY OF ALL EXCAVATION SLOPES, SHEETING, SHORING, TRENCH BOXES, AND ANY OTHER MEANS REQUIRED FOR A SAFE WORK ENVIRONMENT AND FOR PROTECTION OF ADJACENT ROADWAYS AND OTHER STRUCTURES. ALL EXCAVATION WORK SHALL COMPLY WITH THE REQUIREMENTS OF THE FOLLOWING REGULATORY AGENCIES:

   -SUBPART 23-4, "EXCAVATION OPERATIONS", OF NEW YORK DEPARTMENT OF LABOR INDUSTRIAL CODE RULE 23.

   -SUBPART P, "EXCAVATIONS" OF THE UNITED STATES DEPARTMENT OF LABOR OSHA REGULATIONS FOR CONSTRUCTION.

   -ALL OTHER MUNICIPAL, COUNTY, STATE OR FEDERAL AGENCIES, REGULATIONS OR LAWS PERTAINING TO EXCAVATION SAFETY AS MAY APPLY AT THE WORK SITE.

   THE MORE STRINGENT PROVISION IN EACH OF THE ABOVE CODES SHALL APPLY. THESE PROVISIONS SHALL BE CONSIDERED MINIMUM REQUIREMENTS AND SHALL BE INCREASED IF NECESSARY TO PROVIDE SAFE WORKING CONDITIONS.
6. CRUSHED STONE, WHERE CALLED FOR ON THE DRAWINGS, SHALL CONFORM TO NYS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS SECTION 703-02, SIZE DESIGNATION 2.
7. ALL NEW GRADES SHALL BE BLENDED SMOOTHLY WITH EXISTING GRADES TO PROVIDE A SMOOTH TRANSITION BETWEEN NEW GRADING AND EXISTING SURFACES TO REMAIN.
8. ALL SILT FENCES SHALL BE INSTALLED PRIOR TO DISTURBANCE OF EXISTING SITE FINISHES AND SHALL REMAIN IN PLACE UNTIL A STABLE GROWTH OF TURF IS ESTABLISHED.
9. ALL DISTURBED AREAS NOT COVERED WITH SITE IMPROVEMENTS OR LANDSCAPING SHALL BE COVERED WITH 3" OF TOPSOIL AND SEEDED TO ESTABLISH A DENSE GROWTH OF TURF.

REVISIONS		
REV.	DATE	DESCRIPTION
0	2/12/2024	ISSUED FOR PERMITS AND CONSTRUCTION

STATE OF NEW YORK · DEREK C.F. SANDBORN · LICENSED PROFESSIONAL ENGINEER · NO. 076481

SRA ENGINEERS
Evergreen Professional Park
453 Dixon Road, Suite 7, Bldg. 3
Queensbury, New York 12804
(518) 761-0417, FAX: (518) 761-0513

SCALE:	AS NOTED	DRAWN BY:	AMD / RAS
DATE:	2/7/2024	ENG. BY:	AMD / RAS
PROJ. NO:	22-960	CHK'D BY:	ES

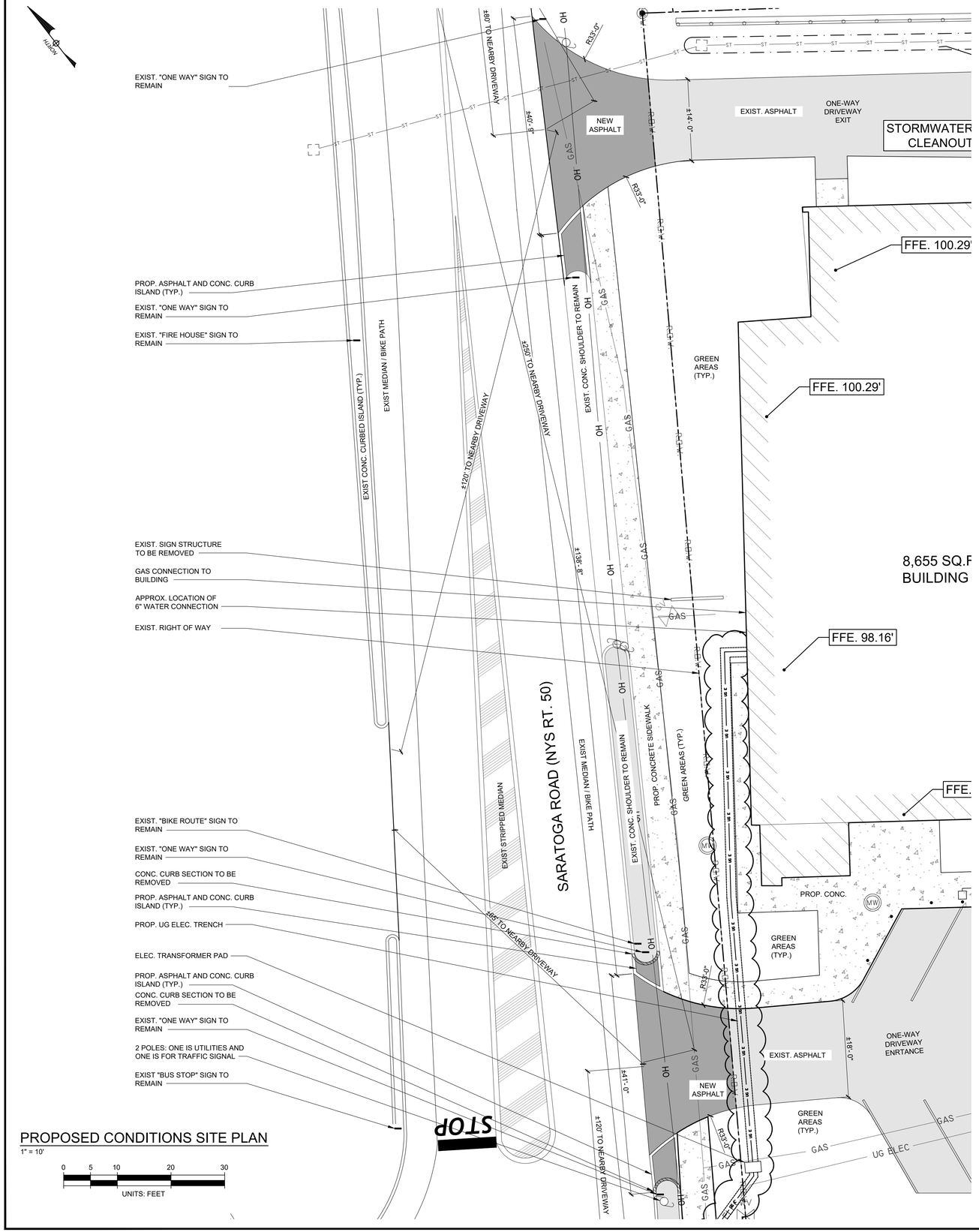
CLIENT NAME
446 SARATOGA ROAD LLC
446 SARATOGA RD
SCHENECTADY, NY, 12302

DRAWING TITLE
PROPOSED CONDITIONS SITE PLAN
446 SARATOGA ROAD
GLENVILLE, NY, 12302

DRAWING NO. C-101

SHT. 2 OF 8

REV. 0



PROPOSED CONDITIONS SITE PLAN

1" = 10'

0 5 10 20 30

UNITS: FEET

WORK ZONE TRAFFIC CONTROL NOTES

1. THERE SHALL BE NO TEMPORARY LANE CLOSURES ON ROUTE 50, MONDAY-FRIDAY 6AM-9PM, SATURDAY 7AM-9PM & SUNDAY 8AM-9PM.
2. LANE CLOSURES AND/OR USE OF TEMPORARY CONCRETE BARRIER WITHIN FIVE FEET OF AN ACTIVE TRAVEL WAY BETWEEN NOVEMBER 1 & APRIL 15 SHALL REQUIRE SPECIAL PERMISSION FROM THE ENGINEER WITH CONCURRENCE FROM THE REGIONAL TRAFFIC OFFICE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER ONE WEEK IN ADVANCE OF ANY REQUESTED CLOSURE.
3. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN AND BICYCLE TRAFFIC THROUGH AND AROUND THE PROJECT FOR THE DURATION OF CONSTRUCTION. MATERIAL, EQUIPMENT OR OTHER BARRIERS SHALL NOT BE PLACED OR MARKED IN SUCH A MANNER AS TO OBSTRUCT PEDESTRIAN OR BICYCLE TRAFFIC OR TO PRESENT A SAFETY HAZARD TO THE NON-MOTORIZED PUBLIC. WHERE PEDESTRIAN TRAFFIC MUST BE RELOCATED OFF THE EXISTING FACILITY, WALKWAYS SHALL BE CLEARLY MARKED AND HAVE A LOGICAL START AND TERMINUS.
4. EXCAVATIONS THAT PRODUCE A DROPP-OFF GREATER THAN 24 INCHES AND WITHIN 10 FEET FROM THE EDGE OF THE TRAVEL LANE THAT WILL REMAIN AT THE END OF THE WORK SHIFT SHALL BE SEPARATED FROM TRAFFIC WITH TEMPORARY CONCRETE BARRIER. SHOULDER AREAS SHOULD BE PREPARED TO RECEIVE THE SHOULDER PAVEMENT MATERIAL IMMEDIATELY AHEAD OF THE SHOULDER PAVING OPERATIONS TO MINIMIZE THE TIME A DROP-OFF EXISTS. THE CONTRACTOR SHALL BEGIN WORK TO ELIMINATE UNPROTECTED DROP-OFFS CREATED BY CONTRACT WORK WITHIN 7 CALENDER DAYS OF COMPLETION OF THE WORK CREATING THE DROPP-OFF.
5. HOLIDAY TEMPORARY LANE/SHOULDER RESTRICTIONS TABLE.

TEMPORARY LANE/SHOULDER CLOSURE RESTRICTIONS FOR HOLIDAYS 2023

HOLIDAY	FALLS ON	TEMPORARY LANE CLOSURES ARE NOT ALLOWED FOR
NEW YEAR'S DAY	SUNDAY JANUARY 1	BEGINNING 6 AM FRIDAY DECEMBER 30, 2022 AND ENDING 6 AM TUESDAY JANUARY 3, 2023
MEMORIAL DAY	MONDAY MAY 29	BEGINNING 6 AM FRIDAY MAY 26, 2023 AND ENDING 6 AM TUESDAY MAY 30, 2023
INDEPENDENCE DAY	TUESDAY JULY 4	BEGINNING 6 AM SATURDAY JULY 1, 2023 AND ENDING 6 AM WEDNESDAY JULY 5, 2023
LABOR DAY	MONDAY SEPTEMBER 4	BEGINNING 6 AM FRIDAY SEPTEMBER 1, 2023 AND ENDING 6 AM TUESDAY SEPTEMBER 5, 2023
THANKSGIVING DAY	THURSDAY NOVEMBER 23	BEGINNING 6 AM WEDNESDAY NOVEMBER 22, 2023 AND ENDING 6 AM MONDAY NOVEMBER 27, 2023
CHRISTMAS DAY	MONDAY DECEMBER 25	BEGINNING 6 AM FRIDAY DECEMBER 22, 2023 AND ENDING 6 AM TUESDAY DECEMBER 26, 2023

TEMPORARY LANE/SHOULDER CLOSURE RESTRICTIONS FOR HOLIDAYS 2024

HOLIDAY	FALLS ON	TEMPORARY LANE CLOSURES ARE NOT ALLOWED FOR
NEW YEAR'S DAY	MONDAY JANUARY 1	BEGINNING 6 AM FRIDAY DECEMBER 30, 2023 AND ENDING 6 AM TUESDAY JANUARY 3, 2024
MEMORIAL DAY	MONDAY MAY 27	BEGINNING 6 AM FRIDAY MAY 24, 2024 AND ENDING 6 AM TUESDAY MAY 28, 2024
INDEPENDENCE DAY	THURSDAY JULY 4	BEGINNING 6 AM WEDNESDAY JULY 3, 2024 AND ENDING 6 AM MONDAY JULY 8, 2024
LABOR DAY	MONDAY SEPTEMBER 2	BEGINNING 6 AM FRIDAY AUGUST 30, 2024 AND ENDING 6 AM TUESDAY SEPTEMBER 3, 2024
THANKSGIVING DAY	THURSDAY NOVEMBER 28	BEGINNING 6 AM WEDNESDAY NOVEMBER 27, 2024 AND ENDING 6 AM MONDAY DECEMBER 2, 2024
CHRISTMAS DAY	WEDNESDAY DECEMBER 25	BEGINNING 6 AM TUESDAY DECEMBER 24, 2024 AND ENDING 6 AM MONDAY DECEMBER 26, 2024

LEGEND

- EXIST. MINOR CONT. (364)
- EXIST. MAJOR CONT. (365)
- PROP. CONTOUR (365)
- PROPERTY LINE
- SILT FENCE
- UNDERGROUND GAS LINE
- UNDERGROUND ELECTRIC LINE
- UNDERGROUND SEWER LINE
- UNDERGROUND STORM WATER LINE
- OVERHEAD UTILITY WIRE
- GUARD RAIL
- SETBACK
- CHAIN LINK FENCE
- TEST PIT LOCATION
- PERC. TEST LOCATION
- PROPOSED SPOT ELEVATION (320.00')
- UTILITY POLE
- GAS VALVE
- MONITOR WELL MANHOLE
- WATER VALVE
- WATER HYDRANT
- SEWER MANHOLE
- CATCH BASIN
- E.V. CHARGE STATION

REVISIONS		
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ERIK C.F. SANDBERG — NO. 076491 — LICENSED PROFESSIONAL ENGINEER — STATE OF NEW YORK

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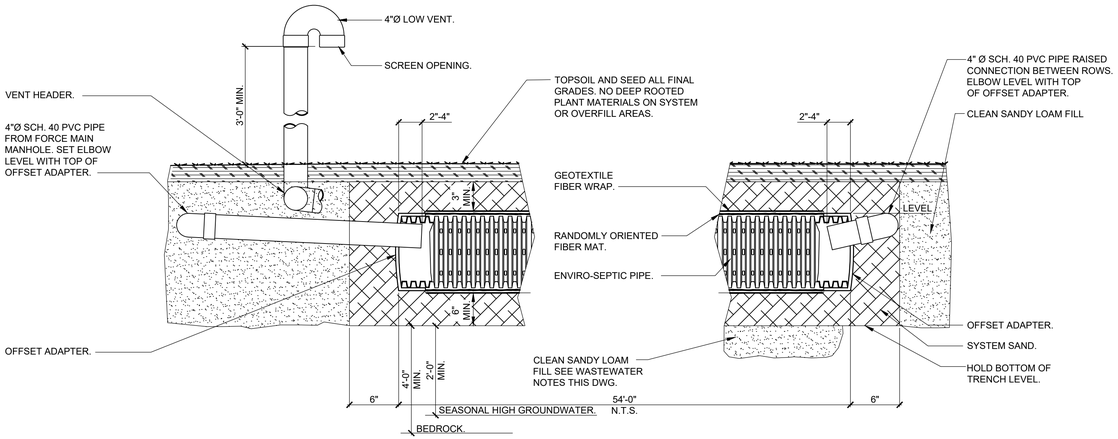
446 SARATOGA RD
SCHENECTADY, NY, 12302

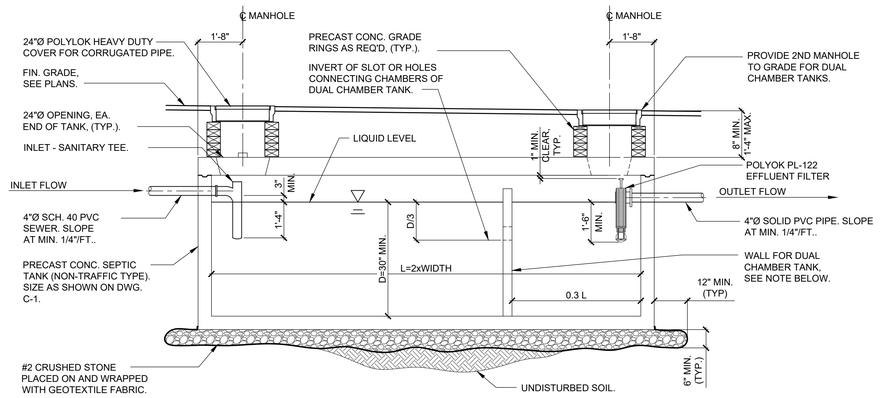
DRAWING TITLE
PROPOSED CONDITIONS DOT SITE PLAN
446 SARATOGA ROAD
GLENVILLE, NY, 12302

DRAWING NO. DOT-101 | SHT. 3 OF 8 | REV. 0



TYPICAL ADVANCED ENVIRO-SEPTIC PIPE SECTION

N.T.S.

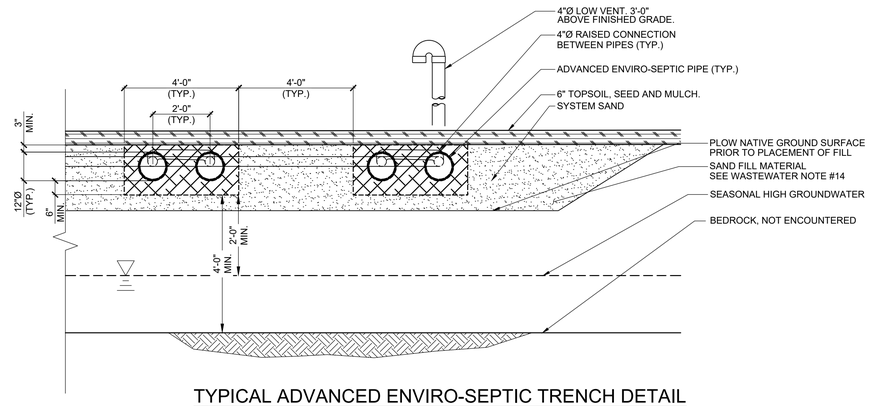


NOTES:

1. PROVIDE FLEXIBLE PIPE BOOTS AT ALL PIPE PENETRATIONS INTO ALL TANKS.
2. PROVIDE DUAL CHAMBER TANK FOR ALL TANKS WITH DIMENSION "L" GREATER THAN OR EQUAL TO 10 FT.. WALL SHALL HAVE A 4"V. x 18" WIDE SLOT, OR (4) 4"Ø HOLES, INVERT LOCATED AS SHOWN.

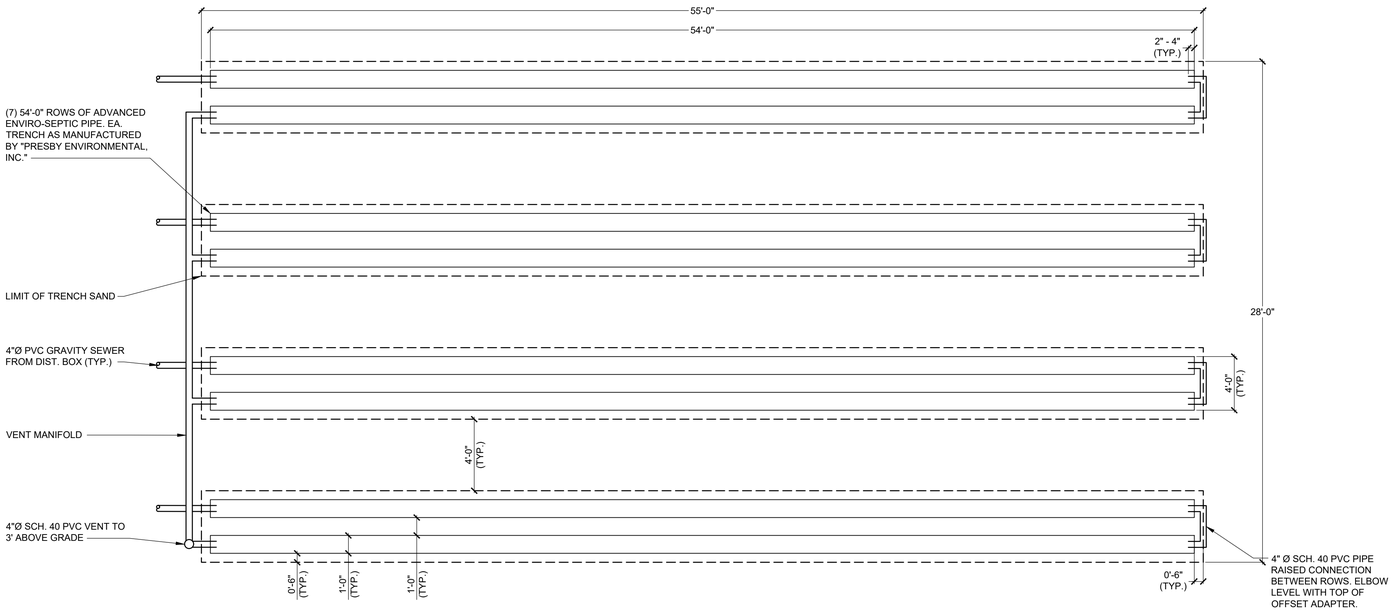
SEPTIC TANK DETAIL

N.T.S.



TYPICAL ADVANCED ENVIRO-SEPTIC TRENCH DETAIL

N.T.S.



TYPICAL ADVANCED ENVIRO-SEPTIC TRENCH PLAN

N.T.S.

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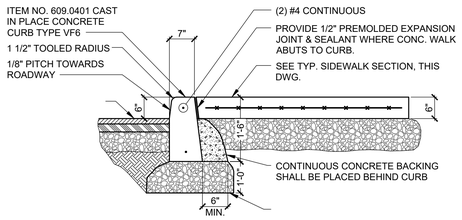
446 SARATOGA RD
SCHENECTADY, NY, 12302

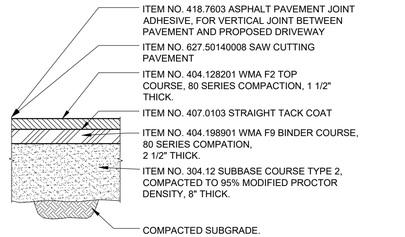
DRAWING TITLE
DETAIL SHEET
446 SARATOGA ROAD
GLENVILLE, NY, 12302

DRAWING NO. C-501
SHT. 4 OF 8
REV. 0



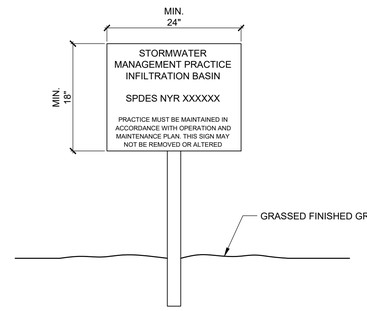
NOTE: CONTRACTOR MAY SUBMIT PRECAST CONC. CURB TO ENGINEER FOR REVIEW IN LIEU OF CAST-IN-PLACE CURB.

TYPICAL CAST-IN-PLACE CONCRETE CURB
N.T.S.

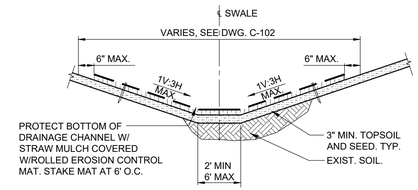


NOTE: REFERENCE "N.Y.S. DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS", JANUARY 1, 2023, FOR MATERIAL TYPE AND CONSTRUCTION REQUIREMENTS.

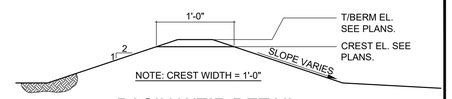
TYPICAL PAVEMENT DETAIL
N.T.S.



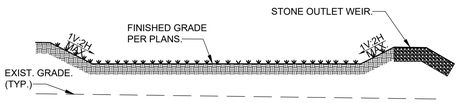
SMP SIGN DETAIL
N.T.S.



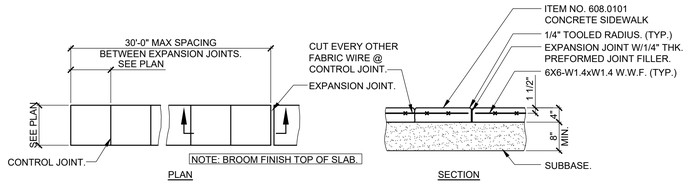
TYPICAL VEGETATED SWALE DETAIL
N.T.S.



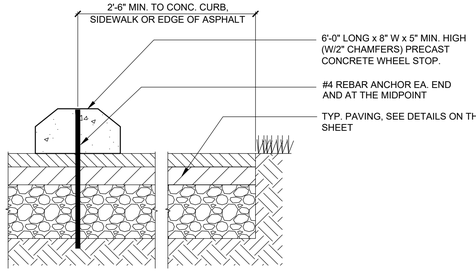
BASIN WEIR DETAIL
N.T.S.



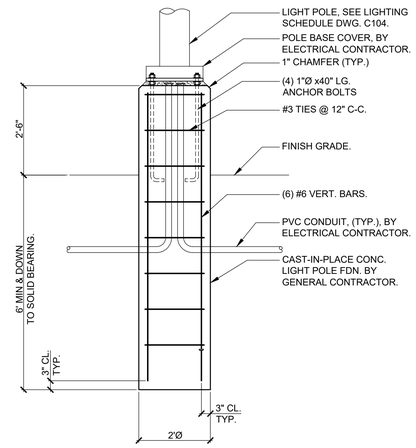
SEDIMENTATION/INFILTRATION BASIN DETAIL
N.T.S.



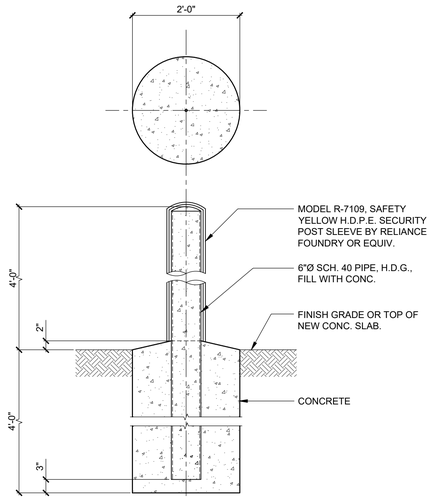
TYPICAL SIDEWALK DETAIL
N.T.S.



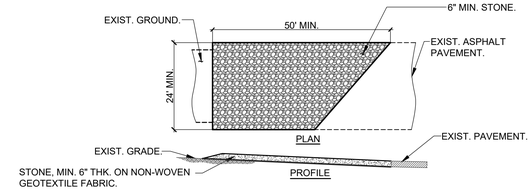
TYPICAL WHEEL STOP DETAIL
N.T.S.



TYPICAL LIGHT POLE FDN. DETAIL
N.T.S.

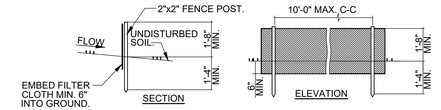


TYPICAL BOLLARD DETAIL
N.T.S.



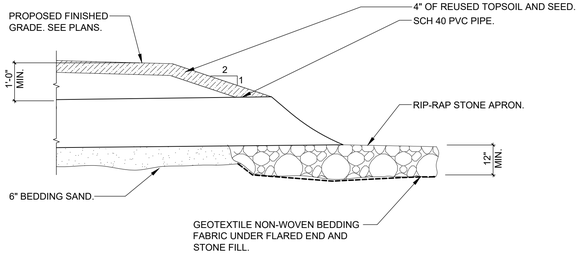
STABILIZED CONSTRUCTION ACCESS

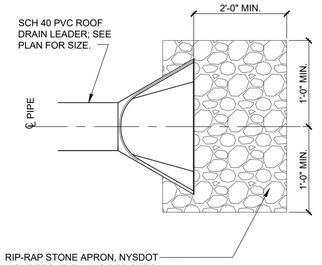
NOTES
1. STONE SHALL CONFORM WITH NYSDOT SPEC. SECTION 703-02 SIZE DESIGNATION 3.
2. GEOTEXTILE FABRIC SHALL BE NON-WOVEN.
3. PERIODICALLY TOP-DRESS ENTRANCE WITH NEW STONE AS SEDIMENT ACCUMULATES. ALL SEDIMENT DROPPED OR WASHED ONTO THE PUBLIC RIGHT-OF-WAY SHALL BE REMOVED IMMEDIATELY



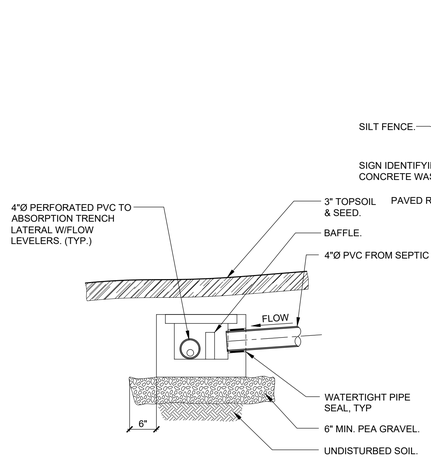
NOTES
1. FILTER CLOTH TO BE FASTENED SECURELY TO POSTS WITH STAPLES OR TIES AT 6" MAX. SPACING.
2. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" AND FOLDED.
3. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE.

SILT FENCE DETAIL
N.T.S.

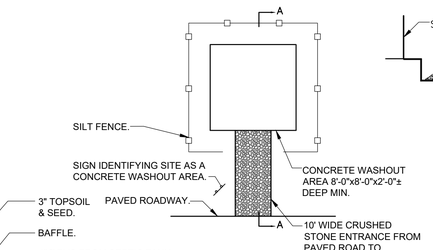




TYPICAL STORM PIPE OUTLET DETAIL
N.T.S.



DISTRIBUTION BOX DETAIL
N.T.S.



NOTES:
1. ALL CONCRETE WASH FACILITIES SHALL BE INSPECTED DAILY. DAMAGED OR LEAKING FACILITIES SHALL BE REPAIRED OR REPLACED IMMEDIATELY.
2. ACCUMULATED HARDENED MATERIAL SHALL BE REMOVED WHEN 75% OF THE STORAGE CAPACITY OF THE STRUCTURE IS FILLED. ANY EXCESS WASH WATER SHALL BE PUMPED INTO A CONTAINMENT VESSEL AND PROPERLY DISPOSED OF OFF SITE.
3. DISPOSE OF HARDENED MATERIAL OFF-SITE IN A CONSTRUCTION/DEMOLITION LANDFILL OR ON-SITE IF INDICATED IN THE APPROVED SWPPP.
4. THE PLASTIC LINER SHALL BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.
5. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL CLEAN OUT AND FILL IN THE WASHOUT AREA TO THE ADJACENT GRADE LEVEL.

CONCRETE WASHOUT AREA
N.T.S.

REVISIONS		
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STATE OF NEW YORK ★ ERIK C.F. SANDBLOM ★ 076481 ★ LICENSED PROFESSIONAL ENGINEER

UNAUTHORIZED ALTERATION OR ADDITION TO THIS DOCUMENT IS A VIOLATION OF SECTION 7209 SUBDIVISION 2 OF THE NEW YORK STATE EDUCATION LAW.

SRA ENGINEERS
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446 SARATOGA ROAD
GLENVILLE, NY, 12302

DRAWING NO. C-502
SHT. 5 OF 8
REV. 0

GENERAL NOTES

1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.

2. PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN SHALL BE PROVIDED, IN WRITING, TO THE DOT ENGINEER FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORK DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.

3. THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS SHALL BE PROVIDED, IN WRITING, TO THE NYSDOT ENGINEER. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.

4. STANDARD SHEET 619-503 MAY BE USED FOR AN OFFSITE DETOUR SETUP FOR BOTH LONG TERM AND SHORT TERM WORK DURATIONS.

5. REGIONAL HIGH-VOLUME RESTRICTIONS SHALL BE FOLLOWED. CONSULT WITH DOT ENGINEER IF EXCEPTION NEEDED.

6. PLAN AHEAD TO AVOID CONFLICTING WORK ZONES. CHECK FOR CONSTRUCTION PROJECTS, CLOSURES, & RESTRICTIONS AT WWW.511NY.ORG, WWW.DOT.NY.GOV/PROJECTS, AND WITH NYSDOT ENGINEER.

7. WORK ZONE INCIDENTS SHALL BE DOCUMENTED AND REPORTED USING EITHER THE DEPARTMENT'S WORK ZONE INCIDENT FORM, OR THE CONSTRUCTION INCIDENT REPORTING SYSTEM, AS APPROPRIATE.

8. CONSIDER CLOSURE WIDTH AND THE ABILITY TO ACCOMMODATE WIDE LOAD VEHICLES BEFORE ESTABLISHING WORK ZONES.

9. IF THE WORK ZONE AFFECTS AN EXISTING ACCESSIBLE AND DETECTABLE PEDESTRIAN FACILITY, ACCESSIBILITY AND DETECTABILITY SHALL BE PROVIDED ALONG THE ALTERNATE ROUTE.

ACTIVITY AREA

1. A 500' MINIMUM LONGITUDINAL DISTANCE SHALL BE MAINTAINED BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

SIGNS

1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.

2. FOR LONG TERM WORK DURATIONS, ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.

3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.

4. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET OR OMITTED WITH THE APPROVAL OF THE DOT ENGINEER. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.

5. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.

6. NYR9-12 SHALL BE USED IN PLACE OF NYR9-11 WHEN A REDUCED REGULATORY SPEED LIMIT SIGN IS AUTHORIZED.

7. RIGID AND FLEXIBLE "ROLL-UP" SIGNS MAY BE USED FOR MOBILE, SHORT DURATION AND SHORT-TERM STATIONARY WORK. RIGID SIGNS MUST BE MOUNTED AT LEAST 5 FEET ABOVE GRADE (7 FEET WHERE THERE ARE PEDESTRIANS OR PARKED CARS). FLEXIBLE SIGNS SHALL BE MOUNTED AT LEAST ONE FOOT ABOVE GRADE. MESH SIGNS SHALL NOT BE USED. USE RETRO REFLECTORIZED RIGID SIGNS FOR NIGHTTIME WORK.

CHANNELIZING DEVICES

1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.

PUBLIC ACCESS

1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.

2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

LANE CLOSURES

1. LANE CLOSURES SHALL BE LOCATED TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.

2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.

3. EACH ARROW PANEL SHALL BE VISIBLE 1500 FEET IN ADVANCE FROM ANY POINT WITHIN THE ROADWAY.

LANE WIDTHS

1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10'.

2. A WRITTEN NOTE SHALL BE PROVIDED TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.

3. IF THE WORK ZONE AFFECTS PEDESTRIANS, A MINIMUM PEDESTRIAN PATHWAY WIDTH OF 5 FEET SHALL BE MAINTAINED UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.

4. TEMPORARY BICYCLE ACCOMMODATIONS SHALL NOT BE LESS THAN WHAT CURRENTLY EXISTS UNLESS AUTHORIZED BY THE ENGINEER.

PROTECTIVE VEHICLES

1. PROTECTIVE VEHICLES ARE DIVIDED INTO 2 CATEGORIES BASED ON THE GROSS VEHICLE WEIGHT (GVW):
- PROTECTIVE VEHICLE LIGHT (PVL) SHALL HAVE A MINIMUM GVW OF 9,500 LBS. OR GREATER.
- PROTECTIVE VEHICLE HEAVY (PVH) SHALL HAVE A MINIMUM GVW OF 22,000 LBS. OR GREATER.

2. IF THE PROTECTIVE VEHICLE ENCROACHES INTO THE TRAVEL LANE, OR IF IT REMAINS ENTIRELY ON THE SHOULDER OF ANY HIGH SPEED ROAD (≥ 45 MPH), IT SHALL BE EQUIPPED WITH A DEPLOYED TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA, SEE TABLE 011-01 ON SHEET 619-011). BALLAST MAY BE USED TO BRING A LIGHTER VEHICLE UP TO THE INDICATED WEIGHT PROVIDED THE BALLAST IS SECURELY CONTAINED WITHIN AN ENCLOSED BODY OR OTHERWISE SECURELY FASTENED TO THE VEHICLE PURSUANT TO FEDERAL MOTOR CARRIER SAFETY ADMINISTRATION (FMCSA) CARGO SECUREMENT RULES, SUCH THAT:
1) THE BALLAST WILL NOT SEPARATE FROM THE VEHICLE UPON IMPACT AND
2) THE BALLAST WEIGHT WILL NOT EXCEED THE MANUFACTURER'S GROSS VEHICLE WEIGHT RATING (GVWR).
TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMIA.

3. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A MOVING OPERATION IT IS REFERRED TO AS A SHADOW VEHICLE(S).

4. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A STATIONARY OPERATION IT IS REFERRED TO AS A BARRIER VEHICLE(S).

5. WHEN A PROTECTIVE VEHICLE IS USED IN ADVANCE OF EITHER MOVING OR STATIONARY OPERATIONS TO DISPLAY SIGN MESSAGES IT IS REFERRED TO AS AN ADVANCE WARNING VEHICLE. WHEN SIGNS ARE MOUNTED ON AN ADVANCED WARNING VEHICLE, THEY SHALL NOT OBSTRUCT VISIBILITY OF ANY LIGHTS (TAILLIGHTS OR WARNING LIGHTS), OR SIDE-VIEW MIRRORS ON THE VEHICLE, OR TRUCK MOUNTED ARROW BOARDS.

6. IN A MOVING OPERATION OR A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR UP TO 1 HOUR, THE OPERATOR SHALL REMAIN IN THE PROTECTIVE VEHICLE WITH THE SAFETY BELT AND HEADREST PROPERLY ADJUSTED, MAINTAIN VEHICLE SPACING, AND KEEP THE WHEELS ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK. THE PARKING BRAKE SHALL BE SET WHENEVER POSSIBLE. TWO-WAY RADIOS SHOULD BE USED TO COMMUNICATE BETWEEN THE OPERATOR AND THE WORK CREW.

7. IN A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR, ONCE THE PROTECTIVE VEHICLE HAS BEEN APPROPRIATELY PLACED, IT SHOULD BE UNOCCUPIED. UNOCCUPIED VEHICLE SHALL BE POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS). THE FRONT WHEELS SHALL BE ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK.

8. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE PROTECTIVE VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE).

9. DIRECT VERBAL COMMUNICATION BETWEEN THE PROTECTIVE VEHICLES AND THE WORK VEHICLE(S) / EQUIPMENT SHALL BE UTILIZED WHERE AVAILABLE.

WORK DURATION DEFINITIONS

1. THERE ARE MAINLY FIVE WORK DURATIONS:

   A. LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.

   B. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.

   C. SHORT-TERM IS STATIONARY DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.

   D. SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N11 NOTES ON NIGHTTIME WORK.

   E. MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY WHERE THE WORK AT ANY SPECIFIC LOCATION COMPLETES WITHIN 15 MINUTES. IT IS USED FOR VEHICLE BASED OPERATIONS AND DOES NOT INVOLVE WORKERS ON FOOT. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N10 NOTES ON NIGHTTIME WORK.

2. SPECIAL OPERATIONS ARE WORK OPERATIONS THAT DO NOT FIT INTO ONE OF THE ABOVE FIVE CATEGORIES. SPECIAL OPERATIONS INCLUDE:

   A. STOP AND GO OPERATIONS - WORK THAT COMPLETES WITHIN 5 MINUTES AND ALLOWS WORKERS ON FOOT.

   B. OTHER OPERATIONS INCLUDING MOWING, MULCHING/HERBICIDE OPERATIONS, TEMPORARY ROAD/INTERSECTION CLOSURES, ETC.

ROADWAY TYPE DEFINITIONS

1. FREEWAY:

   A. INTERSTATE: INTERREGIONAL HIGH-SPEED, HIGH-VOLUME, DIVIDED FACILITIES WITH COMPLETE CONTROL OF ACCESS.

   B. PARKWAY: DIVIDED HIGHWAYS FOR NON-COMMERCIAL TRAFFIC WITH FULL CONTROL OF ACCESS, GRADE PARKWAY SEPARATIONS, INTERCHANGES, AND OCCASIONAL AT-GRADE INTERSECTIONS. PARKWAYS ARE DESIGNATED BY LAW.

2. EXPRESSWAY: DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS. ALL FREEWAY STANDARD SHEETS ARE APPLICABLE TO EXPRESSWAY.

3. NON-FREEWAY:

   A. MULTILANE DIVIDED HIGHWAY

   B. MULTILANE UNDIVIDED HIGHWAY

   C. TWO-LANE TWO-WAY ROADWAY

ALL NON-FREEWAYS CAN BE EITHER URBAN OR RURAL:

URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA)
- •HIGH DENSITY DEVELOPMENT
- •ON-STREET PARKING
- •VARIED BUILDING SETBACKS
- •MULTI-STORY AND LOW-TO MEDIUM-RISE STRUCTURES FOR RESIDENTIAL
- •COMMERCIAL, AND EDUCATIONAL USES, STRUCTURES THAT ACCOMMODATE MIXED USES: COMMERCIAL, RESIDENTIAL, AND PARKING
- •LIGHT INDUSTRIAL, AND SOMETIMES HEAVY INDUSTRIAL, LAND USE
- •PROMINENT DESTINATIONS WITH SPECIALIZED STRUCTURES, E.G., LARGE THEATERS, SPORTS FACILITIES OR CONFERENCE CENTERS
- •HIGH LEVELS OF PEDESTRIAN AND BICYCLIST ACTIVITY, WITH NEARLY CONTINUOUS SIDEWALKS AND MARKED CROSSWALKS
- •HIGHER DENSITY OF TRANSIT STOPS AND ROUTES
- •DRIVEWAY DENSITIES GREATER THAN 25 DRIVEWAYS/MILE ON EACH SIDE OF THE ROAD
- •MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS/MILE OR GREATER
- •MAJOR COMMERCIAL DRIVEWAYS
- •HIGH DENSITY OF CROSS STREETS

RURAL: DOES NOT MEET MORE THAN ONE OF THE ABOVE CRITERIA.

NOTES FOR NIGHTTIME OPERATIONS:

N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.

N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.

N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.

N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.

N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.

N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.

N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.

N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.

N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.

N10. SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS. REFER TO SECTION 619-3.19B FOR BALLOON LIGHTING REQUIREMENTS.

N11. FLAGGERS SHALL USE A FLASHLIGHT WITH RED GLOW CONE/RED LED BATON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.

NEW YORK STATE OF OPPORTUNITY. | Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL GENERAL NOTES

APPROVED DECEMBER 21, 2022

Robert Limoges

ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 22-033

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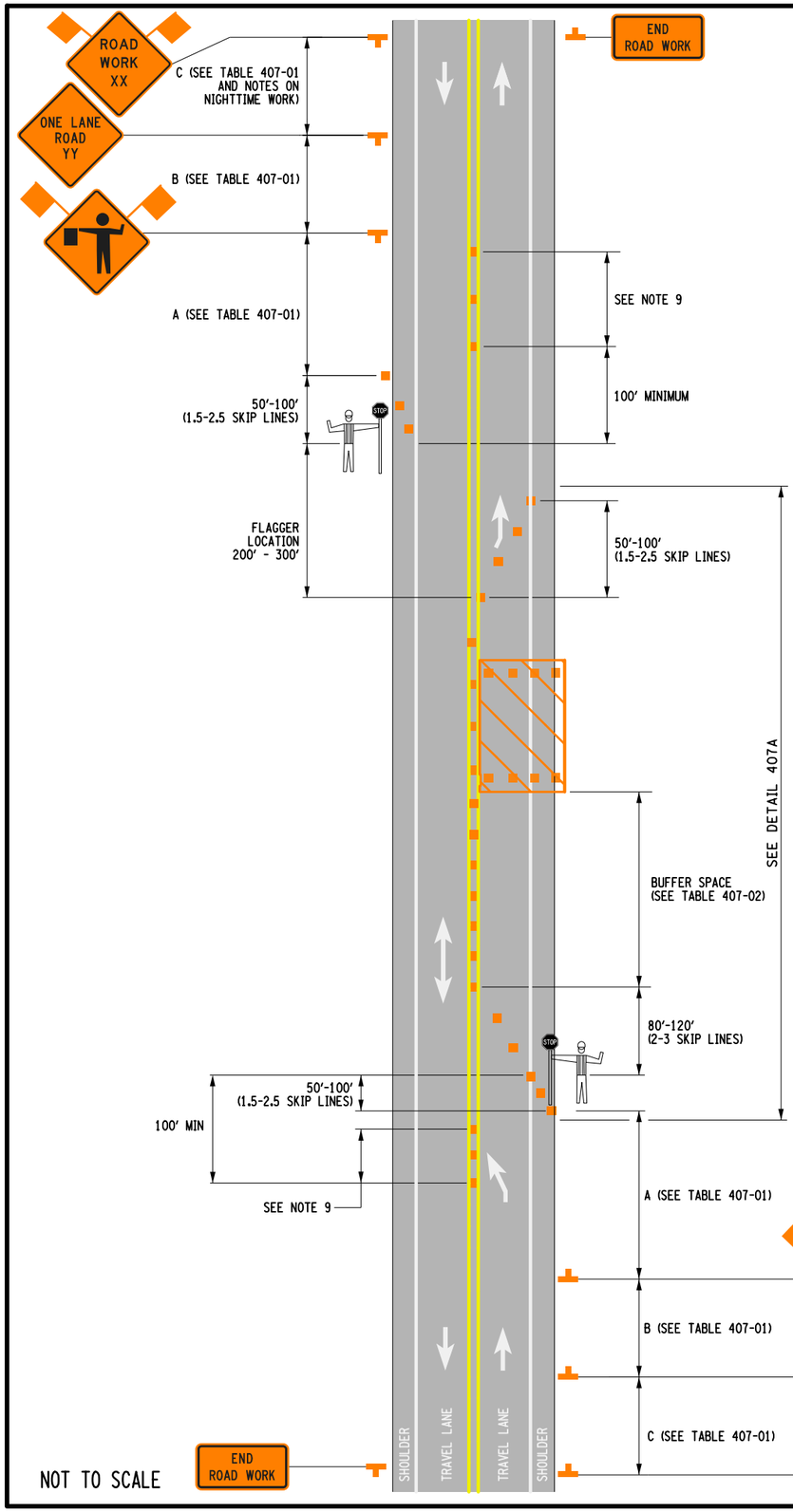

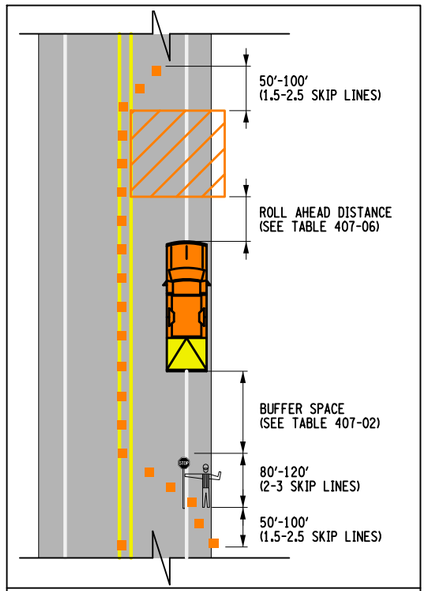

DETAIL 407A: SEE NOTE 6

W3-4
(SEE NOTE 5)

G20-2 THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSTREAM TAPER

W20-7a

NYR9-11
(SEE NOTE 10)

W20-4

WORK AREA

W20-1

FLAGGER

24000 LB PROTECTIVE VEHICLE WITH TMIA SEE TABLE 407-05

CHANNELIZING DEVICE (SEE TABLE 407-03)

NOTES:

1. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.

2. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.

3. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.

4. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.

5. IF THE TRAFFIC IS EXPECTED TO QUEUE PAST THE W20-4 SIGN, A W3-4 SIGN SHOULD BE ADDED HALFWAY BETWEEN THE W20-4 AND W20-1 SIGNS.

6. IF CONDITION WARRANTS, PROTECTIVE VEHICLE WITH APPROPRIATE ROLL AHEAD DISTANCE MAY BE USED IN ADVANCE OF THE WORK AREA. TO USE PROTECTIVE VEHICLE, BUFFER SPACE SHALL BE PROVIDED ACCORDINGLY, AND THE WHEELS SHALL BE ALIGNED WITH THE LANE STRIPING.

7. FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.

8. ALL FLAGGERS SHALL USE 24" (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6' STAFF. THE PADDLE IS THE PREFERRED DEVICE, BUT THE FLAG MAY BE USED AT INTERSECTIONS WHERE THE STOP/SLOW PADDLE WOULD OFFER CONTRADICTING INFORMATION TO DRIVERS TRAVELING IN OPPOSITE DIRECTIONS/LEGS OF THE INTERSECTION OR DURING INCIDENT MANAGEMENT SITUATIONS.

9. CENTERLINE CONES MAY BE ADDED TO ENHANCE THE VISIBILITY OF THE FLAGGER STATION. IF CONES ARE USED, PLACE THEM 100 FT. (MINIMUM) FROM FLAGGER.

10. THE NY9-11 SIGN IS RECOMMENDED. WHEN USED, IT SHALL BE PLACED IN ADVANCE OF THE FIRST ADVANCE WARNING SIGN. THE PLACEMENT DISTANCE SHALL BE 1000' FOR POSTED SPEED LIMITS OF 45 MPH OR HIGHER, AND 300' - 500' FOR POSTED SPEED LIMITS OF LESS THAN 45 MPH.

NOTES ON NIGHTTIME WORK:

N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.

N2. ALL SIGNS, STOP / SLOW PADDLES AND RED FLAGS USED TO WARN / ALERT / CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.

N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMET AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.

N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS AT ALL TIMES.

N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.

N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS,INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.

N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.

N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.

N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL.

N10. SEE NIGHTTIME SAFETY BULLETIN, HDM §16.5.7, & STANDARD SPECIFICATIONS §619 FOR ADDITONAL REQUIREMENTS AND CONSIDERATIONS.

REFER TO SHEET 2 OF 2 FOR ALL TABLES

NEW YORK STATE OF OPPORTUNITY. Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY LANE CLOSURE WITH FLAGGERS INTERMEDIATE TERM OPERATIONS (SHEET 1 OF 2)

APPROVED DECEMBER 2, 2021

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 21-028

619-407

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TABLE 407-01: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH•)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH•)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH•)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.

• PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 407-02: LONGITUDINAL BUFFER SPACE

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.)/ * OF SKIP LINES
45	360/9
50	425/11
55	495/13
65	645/16

TABLE 407-03: CHANNELIZING DEVICE APPLICATION FOR INTERMEDIATE-TERM STATIONARY WORK ZONES

WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR										
SHOULDER/MERGING/ SHIFTING TAPERS	20 FT. •	X							X	
	40 FT.	X							X	
MARKING FOR TRANSVERSE BUMPS[1]	N/A	X[2]			X[2]				X[2]	
TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER	800 FT.	X		X	X			X	X	O
REMOVAL OF EXISTING GUIDE RAIL	80 FT.	X		X	X	X		X	X	O
	40 FT.									

NOTES: X= ALLOWED, BLANK = NOT ALLOWED, O = OPTIONAL
1. - A TYPE 1 OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.
2. - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.

• SEE NOTE 3 ON SHEET 1 OF 2.

TABLE 407-04: REQUIRED SIGN SIZES•

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
W3-4	36x36	48x48
W20-1	36x36	48x48
W20-4	36x36	48x48
W20-7a	36x36	48x48
WARNING FLAG	18x18	18x18

•FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

TABLE 407-05: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
	EXPOSURE CONDITIONS[1]			
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	SEE NOTE 2	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	SEE NOTE 2	SEE NOTE 2	SEE NOTE 2

LEGEND

P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY

TMIA: TMIA REQUIRED

NOTES:
1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

TABLE 407-06: ROLL AHEAD DISTANCE

ROLL AHEAD DISTANCE (FT.)/* OF SKIP LINES FOR VEHICLES

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

NEW YORK STATE OF OPPORTUNITY. Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY LANE CLOSURE WITH FLAGGERS INTERMEDIATE TERM OPERATION (SHEET 2 OF 2)

APPROVED APRIL 8, 2022

Robert Limoges

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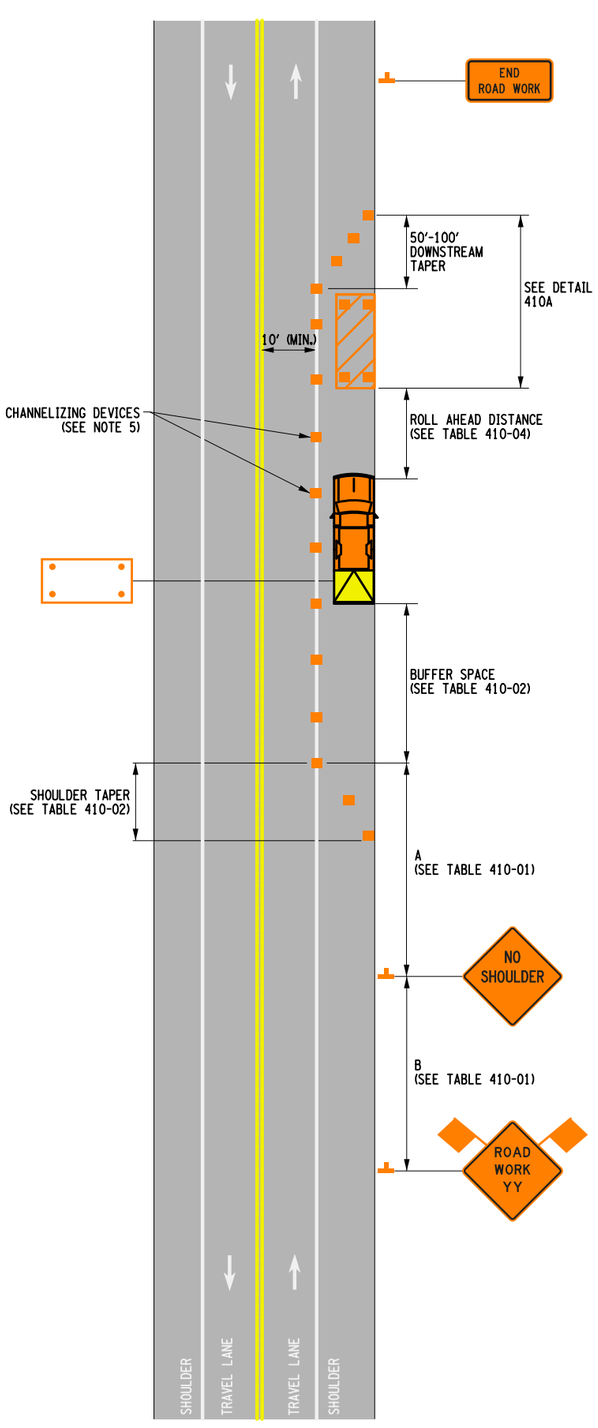

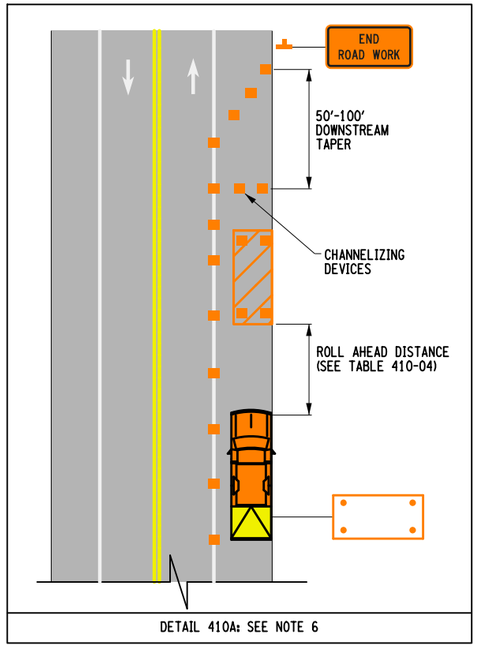

DETAIL 410A: SEE NOTE 6

NOTES:

1. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.

2. NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.

3. XX IS THE EXPECTED OVERALL LENGTH OF THE OPERATION TO BE COMPLETED WITHIN THE WORK DAY. A SUPPLEMENTAL DISTANCE PLAQUE W7-3a SHALL BE USED WITH SIGN W20-1 WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK MAY BECOME GREATER THAN 2 MILES AS A RESULT OF THE FOLLOWING SITUATIONS:
   - MULTIPLE WORK LOCATIONS ARE ANTICIPATED WITHIN XX MILES FROM THE W20-1 SIGN
   - WORK AREA WILL BE RELOCATED DURING THE DURATION OF THE WORK WITHIN XX MILES FROM THE W20-1 SIGN

THE SUPPLEMENT SIGN W7-3a SHALL INDICATE THE MAXIMUM ANTICIPATED DISTANCE BETWEEN THE W20-1 SIGN AND THE FARTHEST WORK LOCATION.

4. WHEN MULTIPLE WORK LOCATIONS EXIST WITHIN XX MILES FROM THE W20-1 SIGN, A G20-1 SIGN SHALL BE PLACED EVERY TWO MILES INDICATING THE DISTANCE FROM THE SIGN TO THE FARTHEST WORK LOCATION.

5. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.

6. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.

7. THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE, BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.

NOTES ON NIGHTTIME WORK:

N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.

N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.

N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.

N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.

N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.

N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.

N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.

N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.

N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL.

N10. SEE NIGHTTIME SAFETY BULLETIN, HDM §16.5.7, & STANDARD SPECIFICATIONS §619 FOR ADDITONAL REQUIREMENTS AND CONSIDERATIONS.

G20-2 THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSTREAM TAPER

W8-23

W20-1

W7-3a (SEE NOTE 3)

G20-1 (SEE NOTE 4)


24000 LB PROTECTIVE VEHICLE WITH TMIA SEE TABLE 410-03


WORK AREA

CHANNELIZING DEVICE (SEE TABLE 410-05)


ARROW PANEL (CAUTION MODE)

REFER TO SHEET 2 OF 2 FOR ALL TABLES

NEW YORK STATE OF OPPORTUNITY. Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY SHOULDER CLOSURE INTERMEDIATE TERM OPERATION (SHEET 1 OF 2)

APPROVED DECEMBER 2, 2021

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 21-028

619-410

NOT TO SCALE

FILE NAME = 619-410-1.dgn
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TABLE 410-01: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS	
	A (FT.)	B (FT.)
URBAN (≤ 30 MPH•)	100	100
URBAN (35-40 MPH•)	200	200
URBAN (≥ 45 MPH•)	350	350
RURAL	500	500
• PRECONSTRUCTION POSTED SPEED LIMIT		

TABLE 410-02: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.)/ * OF SKIP LINES	TAPER LENGTH: L (FT.)/ * OF SKIP LINES/ * OF CHANNELIZING DEVICES			SHOULDER TAPER LENGTH: L/3 (FT.)/ * OF SKIP LINES/ * OF CHANNELIZING DEVICES		
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)			FOR SHOULDER WIDTH		
		10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5

TABLE 410-03: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
	EXPOSURE CONDITIONS[1]			
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	SEE NOTE 2	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	SEE NOTE 2	SEE NOTE 2	SEE NOTE 2

LEGEND

P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY

TMIA: TMIA REQUIRED

NOTES:
1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

TABLE 410-04: ROLL AHEAD DISTANCE

ROLL AHEAD DISTANCE (FT.)/* OF SKIP LINES FOR VEHICLES

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

TABLE 410-05: CHANNELIZING DEVICE APPLICATION FOR INTERMEDIATE-TERM STATIONARY WORK ZONES

WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
SHOULDER/MERGING/ SHIFTING TAPERS	20 FT. •	X							X	
	40 FT.	X							X	
MARKING FOR TRANSVERSE BUMPS[1]	N/A	X[2]	X		X[2]				X[2]	
TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER	800 FT.	X		X	X			X	X	O
REMOVAL OF EXISTING GUIDE RAIL	80 FT.	X		X	X	X		X	X	O
	40 FT.									

NOTES: X= ALLOWED, BLANK = NOT ALLOWED, O = OPTIONAL
1. - A TYPE 1 OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.
2. - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.

• SEE NOTE 5 ON SHEET 1 OF 2.

TABLE 410-06: REQUIRED SIGN SIZES•

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
W20-1	36x36	48x48
W21-5	36x36	48x48
W7-3a	24x18	36x30
G20-1	36x18	48x24
WARNING FLAG	18x18	18x18

•FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

NEW YORK STATE OF OPPORTUNITY. Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY SHOULDER CLOSURE INTERMEDIATE TERM OPERATION (SHEET 2 OF 2)

APPROVED APRIL 8, 2022

Robert Limoges

ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 22-008

619-410

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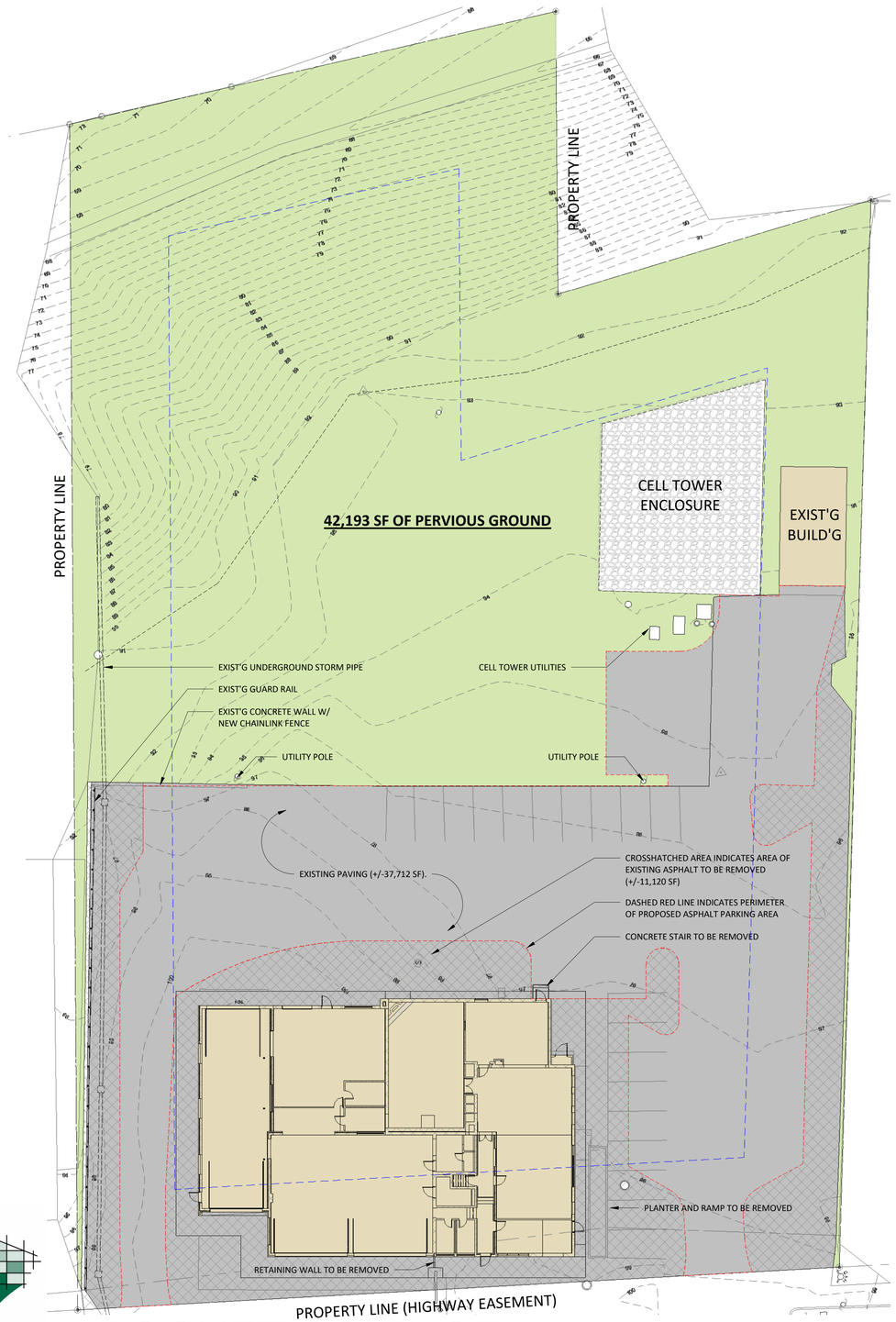
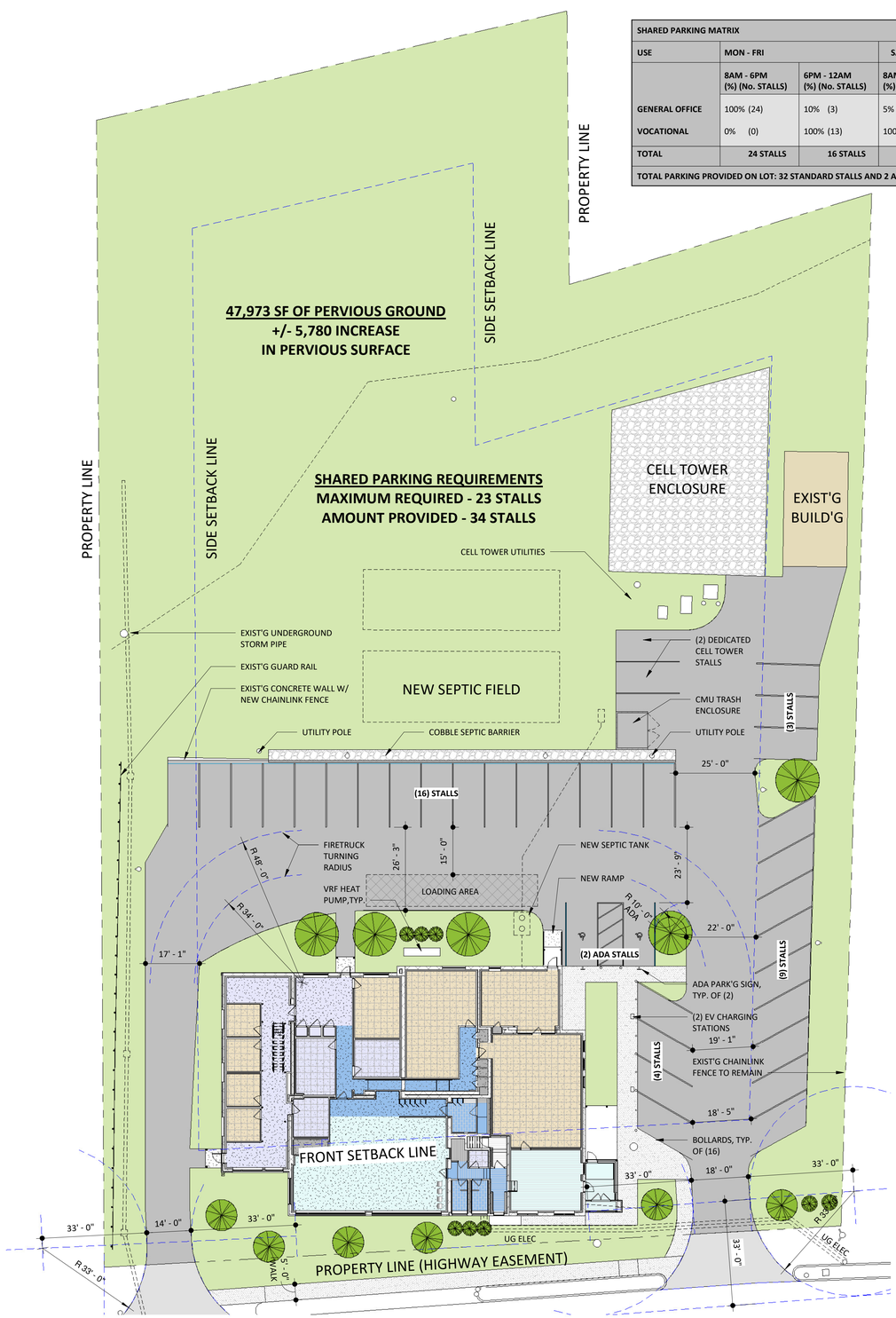


SHARED PARKING MATRIX				
USE	MON - FRI		SAT - SUN	
	8AM - 6PM (%) (No. STALLS)	6PM - 12AM (%) (No. STALLS)	8AM - 6PM (%) (No. STALLS)	6PM - 12AM (%) (No. STALLS)
GENERAL OFFICE	100% (24)	10% (3)	5% (2)	5% (2)
VOCATIONAL	0% (0)	100% (13)	100% (13)	0% (0)
TOTAL	24 STALLS	16 STALLS	15 STALLS	2 STALLS
TOTAL PARKING PROVIDED ON LOT: 32 STANDARD STALLS AND 2 ADA STALLS (34 TOTAL STALLS)				



ROCKET SCIENCE

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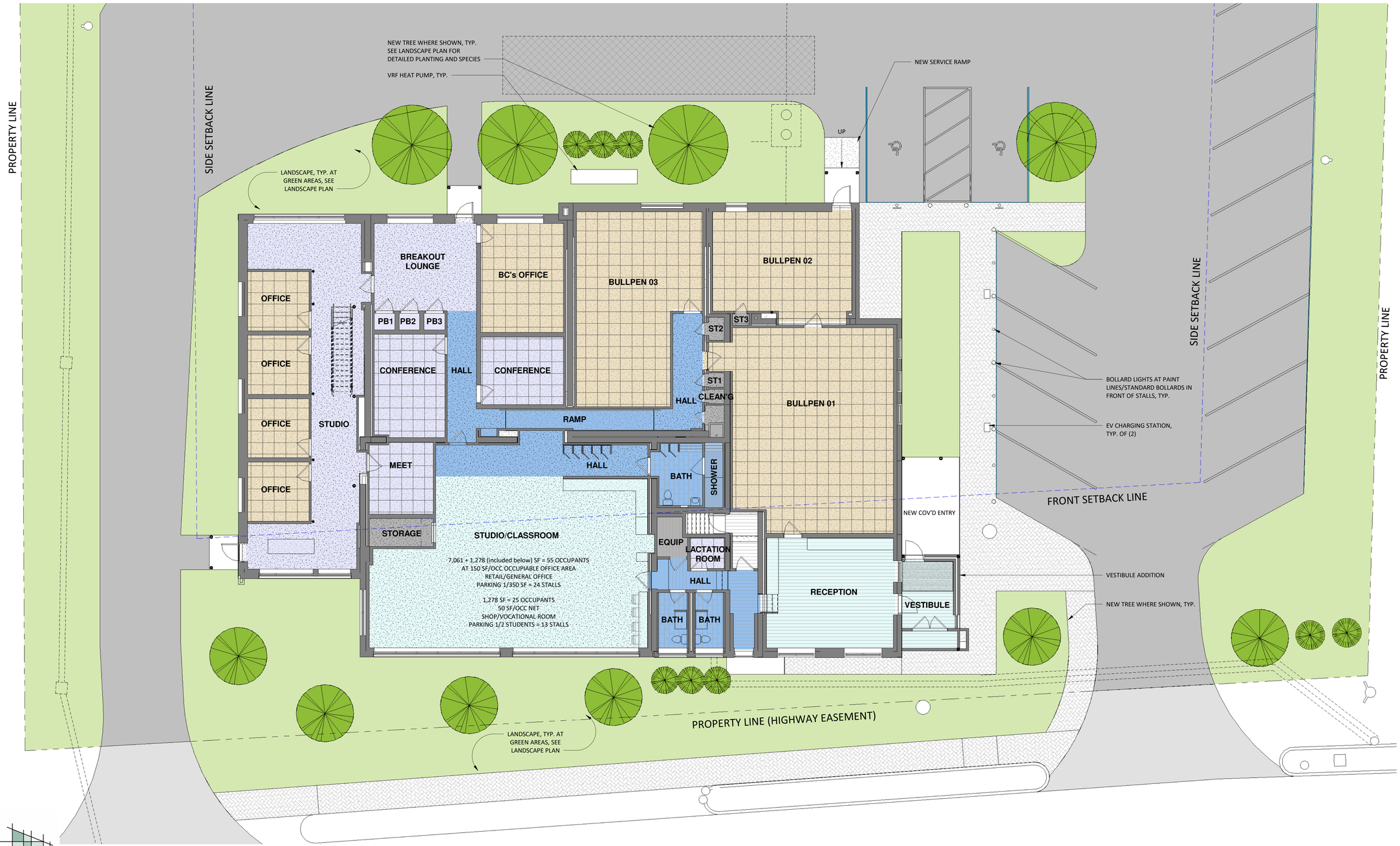

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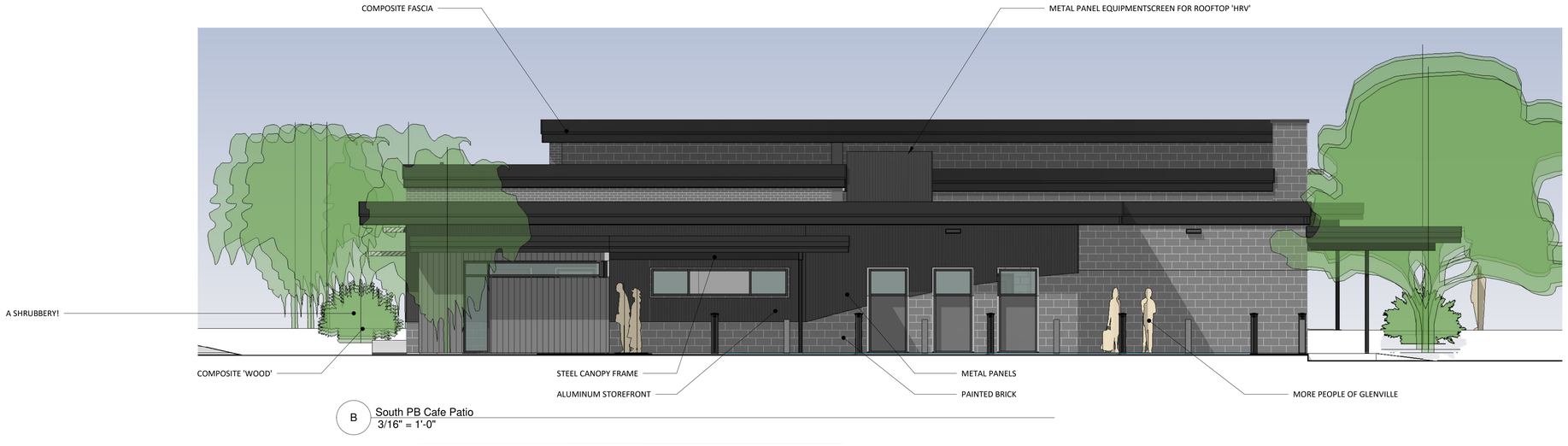
A West PB Saratoga Road
3/16" = 1'-0"



B South PB Cafe Patio
3/16" = 1'-0"

ROCKET SCIENCE

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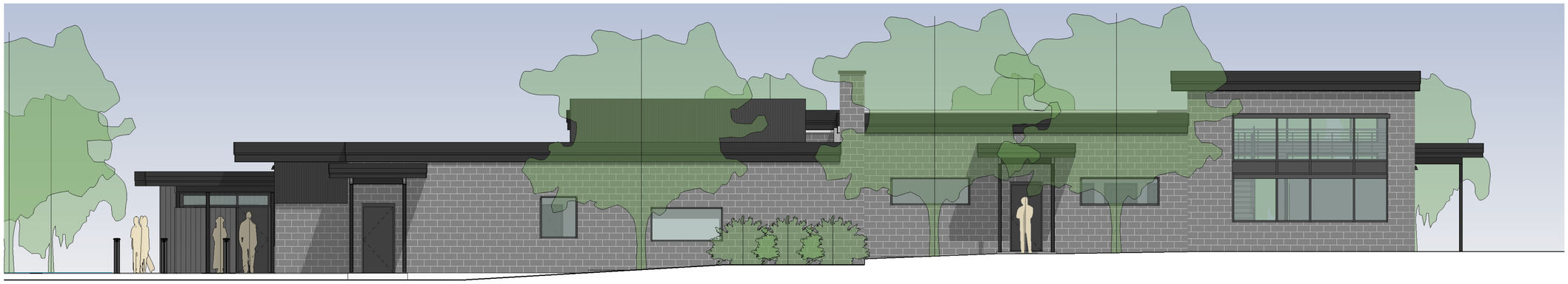
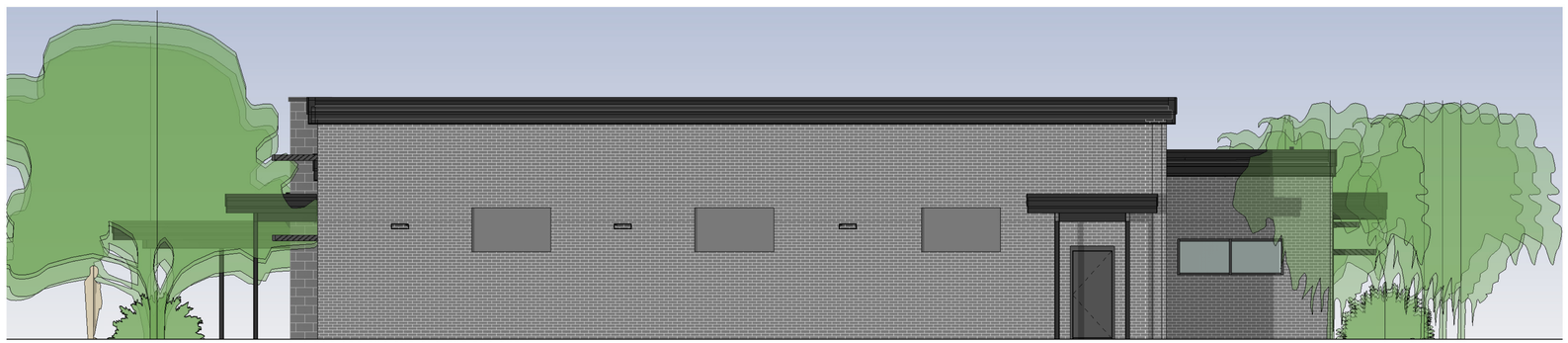


C East PB
3/16" = 1'-0"

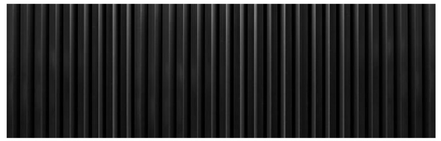
BLACK STOREFRONT GLAZING

BLACK STEEL AND WOOD ENTRY CANOPIES AND SHADE PERGOLA

WEATHERED GRAY COMPOSITE 'WOOD' RAINSCREEN



D North PB
3/16" = 1'-0"



BLACK CURROGATED METAL CLADDING

DARK GRAY STAINED AND PAINTED BRICK AND CMU

ROCKET SCIENCE

PB 202

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